

**COUNTY OF EL DORADO DEVELOPMENT SERVICES  
PLANNING COMMISSION  
STAFF REPORT**



**Agenda of:** July 11, 2013  
**Item No.:** 8.c  
**Staff:** Tom Dougherty

**PLANNED DEVELOPMENT**

**FILE NUMBER:** PD12-0003/Green Valley Convenience Center

**APPLICANT:** Strauch Companies

**AGENT:** Barghausen Consulting Engineers, Inc.

**PARCEL OWNER:** Cemo Family Properties

**REQUEST:**

1. Development Plan to allow the construction of a gas station, convenience store, drive-through fast-food restaurant, single-bay self-service carwash; and
2. Finding of Consistency with General Plan Policy 7.3.3.4 to allow a reduction of the wetland setback from 50 feet to 10 feet.

**LOCATION:** Southeast corner of the intersection of Green Valley Road and Sophia Parkway, in the El Dorado Hills area, Supervisorial District 1. (Exhibit A).

**APN:** 124-301-46 (Exhibit B)

**ACREAGE:** 2.12 acres

**GENERAL PLAN:** Commercial (D) (Exhibit C)

**ZONING:** Commercial-Planned Development (C-PD) (Exhibit E)

**ENVIRONMENTAL DOCUMENT:** Mitigated Negative Declaration

**RECOMMENDATION:** Staff recommends the Planning Commission take the following actions:

1. Adopt the Mitigated Negative Declaration based on the Initial Study prepared by staff;
2. Adopt the Mitigation Monitoring Program in accordance with CEQA Guidelines, Section 15074(d), as incorporated in the Conditions of Approval and Mitigation Measures in Attachment 1;
3. Approve Planned Development PD12-0003, based on the Findings in Attachment 2 and subject to the Conditions of Approval in Attachment 1; and
4. Approve the Finding of Consistency with General Plan Policy 7.3.3.4 to allow a reduction of the wetland setback from 50 feet to 10 feet.

## **STAFF ANALYSIS**

**Project Description:** Development Plan to allow construction of the following:

- a. 4,602 square foot open-sided canopy w/8 self-service fuel pumps;
- b. 2,773 square foot convenience store;
- c. 2,183 square foot fast food restaurant with a drive-through;
- d. 1,196 square foot single-bay self-service carwash;
- e. 171 square foot trash enclosure;
- f. 20'-8.75" tall, 79.9 square-foot monument site identification sign; and
- g. 12-foot tall retaining wall.

**Site Description:** The 2.12-acre parcel is located between 400 and 420 feet elevation above sea level. The area of the proposed construction is currently a vacant lot with frontage on Green Valley Road to the north and Sophia Parkway to the west. The site is approximately 10 feet below the adjacent roadway grades of Sophia Parkway and Green Valley Road. The parcel contains old piles of soil spoils, with gravel and cobble evident at the surface, and covered primarily with non native grasses and herbaceous plants typical of frequently disturbed (ruderal) sites. There are a few young cottonwood and willow trees which have established themselves among the spoils piles and along the stream, along with blackberry bushes. The site is triangular in shape with an approximate width of 200 feet and length of 600 feet. A 15-foot-wide utility easement runs along the east boundary. The site drains to the intermittent stream that bisects the parcel and flows in an east to west direction. The stream continues westward under Sophia Parkway through a culvert system consisting of three 48-inch-diameter reinforced concrete pipes and headwall and empties into the Mormon Island Wetland Preserve, which is located on the west side of Shadowfax Lane. The Mormon Island Dam, one of the dams containing Folsom Lake, is located approximately 1,400 feet to the northwest across Green Valley Road.

The site contains an existing asphalt drive apron and unsurfaced road at the northeast corner of the site. The project proposes to develop approximately 1.3 acres of the site and will leave the

0.8-acre balance undisturbed. The developed site will add approximately 0.95 acre of impervious surfaces and add approximately 0.39 acre of landscaping.

**Parcel Creation:** On January 13, 2009 the Board of Supervisors approved a zone change and parcel map (Z07-0034/P07-0033) that changed the zoning from Two-Acre Residential (R2A) to Commercial- Planned Development (C-PD) and created three parcels: one on the east side of Sophia Parkway and two on the west. The subject parcel is Parcel 2 of that Parcel Map (PM50-82), provided in this document as Exhibit C.

**Project Issues:** The primary issues with this project are aesthetics, air quality, traffic/circulation, drive-thru lane evaluations, and impacts to the intermittent stream. Other discussion items include building elevations/materials, fire protection, grading and drainage; landscaping, land use compatibility, lighting; parking; signs, noise, and sewage disposal and water. The following sections are summaries of the more detailed analysis' contained in the Proposed Mitigated Negative Declaration and Initial Study, included as Attachment Q.

### **Primary Issues**

**Aesthetics:** The site is surrounded by developed or vacant commercial land except to the east. Two medium density residential lots abut a portion of the property, and high density residential lots are adjacent at the southeast corner. (See Exhibits D and E.) The project design, proposed constructions materials, and colors of the physical elements, were analyzed for consistency with the Community Design Guide and surrounding commercial businesses. With the exception of the height of the proposed monument sign, and signs facing south and east, the project is substantially consistent with the design of other commercial projects that have been approved along Green Valley Road between Salmon Falls Road and the El Dorado/Sacramento county line. Additionally, conditions of approval are recommended to assure the rooftop mechanical equipment would be shielded from views by parapet walls, landscaping meets County standards, and outdoor lighting conforms to proposed plans and County standards (Conditions 10, 12 and 13).

The proposed retaining wall would separate the wetland preserve from the development, and would be buffered from views from the south by the proposed and existing riparian vegetation and landscaping. The project would continue to provide the visual character of the riparian area that currently exists by keeping ten feet north of the streambed high water mark areas and those southward essentially intact post construction. The landscape plan includes evergreen species (deodar cedar, holly oak, ponderosa pine) on the south and east side to buffer views into the project from the east and south sides (Landscape Planting Plan, Sheet L1 of 5, Exhibit I).

**Air Quality:** An Air Quality Analysis dated December 4, 2012, was submitted for the project. The El Dorado County Guide to Air Quality Assessment (CEQA Guide; El Dorado County 2002) was used to evaluate the proposed commercial development. The Analysis determined that there would be potential short-term impacts during construction and long-term impacts during operations. Three recommended mitigation measures have been included in Attachment 1 to reduce potential impacts to air quality. The El Dorado County Air Quality Management District recommended conditions of approval to ensure AQMD air quality rules are implemented

for the project. With conditions and mitigation measures, the project would be compliant with District standards for air quality. Air Quality is discussed in more detail in the Air Quality section of the draft Mitigated Negative Declaration and Initial Study.

**Traffic/Circulation:** The project proposes two new encroachments, one each onto Green Valley Road and Sophia Parkway. These encroachments would be right in and right out only due to safety concerns for left turn movements. Access and circulation driveways have been analyzed by the Transportation Division and the El Dorado Hills Fire Department and found by both to be adequate for interior circulation. Encroachment improvements would be designed to meet County standards, and a condition is recommended to modify the corner to accommodate U-turn movements from west-bound Green Valley Road to access the gas station.

The ARCO AM/PM Gas Station Traffic Impact Study, dated May 23, 2013, by KD Anderson & Associates indicated the project would cause an increase in traffic on area roadways and intersections of approximately 1,480 net new daily trips on a weekday basis, with 113 A.M. peak hour trips and 125 P.M. peak hour trips. This project would add to traffic at the Green Valley Road/Francisco Drive, Green Valley Road/El Dorado Hills Blvd., and El Dorado Hills Blvd./Francisco Drive intersections. Policies under General Plan Objective TC-X require that projects that worsen traffic construct or ensure funding is available to construct improvements to maintain level of service (LOS) standards.

The El Dorado Hills Boulevard/Francisco Drive intersection is presently operating at LOS F and queue lengths currently exceed available lengths at Green Valley Road/El Dorado Hills Blvd. The County has included improvements to the first intersection in its Capital Improvement Program (CIP 71358 Francisco Drive Right-Turn Pocket.) It is planned to be constructed in the 2013-14 fiscal year. CIP 72332, the El Dorado Hills Boulevard/Francisco Drive Intersection Alignment project, is presently unfunded but is included in the 20-year C.I.P. The queuing issue will be resolved with the improvements to Green Valley Road planned with CIP GP159, Green Valley Road Widening from Salmon Falls Road to Deer Valley Road. The project will be required to pay Traffic Impact Mitigation (TIM) fees based on the formula adopted by the County for fuel pumps, a convenience store, fast food restaurant and carwash. Based on the current fee schedule, the fees would be in the range of \$200,000 or more. The payment of fees would be considered their fair share of the impact improvements. Since the road improvements necessary to serve this project are included in the 20-year CIP, the project is consistent with applicable General Plan policies.

Encroachment and frontage improvements and the payment of TIM fees satisfy the General Plan and design requirements to ensure safe ingress and egress and maintain LOS standards at all applicable roadways and intersections.

**Drive-thru Lane Evaluations:** The Traffic Impact Analysis (TIA) dated May 23, 2013, describes the following details for the drive-thru lane for the restaurant:

The project includes a Quick Service Restaurant (QSR) with drive-through capabilities. A review of internal queuing for the drive-through lane was conducted to determine whether adequate queuing is available without obstructing other movements on site, including ingress and

egress at the driveways. The applicant has indicated that a Schlotzsky's Restaurant will be the operator of the quick service restaurant. Information provided by Schlotzsky's suggested that they require their queuing areas behind the menu board to accommodate at least four vehicles. Figure 14 of the TIA displays the available queue length from the menu board located about 100 feet from the drive-thru entry. Five vehicles can be queued without encroaching into the travel aisle along the west side of the site. The analysis determined that circulation to and from the Sophia Parkway driveway can be completed without blocking vehicle access.

It appears that the drive-thru would be of adequate length to facilitate the potential use by business wishing to use that option. Both DOT and the El Dorado Hills Fire Department have also reviewed both drive-thru proposals for interior vehicular circulation and neither responded with any concerns of the design as proposed.

**Intermittent Stream:** General Plan Policy 7.3.3.4 requires that intermittent streams have a 50-foot setback. The Policy allows that setback to be modified if information is received that demonstrates a different setback is necessary or would be sufficient to protect the waterway.

The Interim Interpretive Guidelines for General Plan Policy 7.3.3.4 are utilized as the guide to implement the Policy. The Biological and Jurisdictional Delineation Report Updates for the Green Valley Convenience Center, dated May 1, 2013, determined there were no isolated wetlands but that the intermittent stream constituted 0.47 acre of potential jurisdictional wetlands. The applicants submitted a biological report required by the Guidelines to support their request. There has been a U.S. Army Corps verification of the wetland delineation.

A mitigation measure has been added requiring a re-vegetation plan for the riparian area. Both the California Department of Fish and Wildlife and Army Corps have reviewed the recommended mitigation measures for the impacts to the stream. Both have responded that the proposed mitigation measures appear to be adequate to protect the stream. Fish and Wildlife would require review of the final restoration and grading plans to make their final determinations during the 1602 processes.

With the inclusion of mitigation measures BIO-2 to 4, to minimize impacts on the intermittent stream, the project could be found to be consistent with the intent of General Plan Policy 7.3.3.4 and the Interim Interpretive Guidelines for that Policy. The area to be filled north of the stream has historically been disturbed with earth-moving equipment and soil spoils piles. The riparian vegetation is limited north of the stream. The area south of the stream has been determined to have a greater potential for the establishment of a riparian area. The project would be required to fulfill the mitigation measures and Best Management Practices would be included in the grading and restoration processes. The restoration plan would enhance the existing riparian area, and still would allow reasonable use of the 2.12-acre parcel that has been designated by the General plan for commercial uses. The intermittent stream is discussed in more detail in the Biological Resources section of the proposed Mitigated Negative Declaration and Initial Study.

## Other Issues

**Building Elevations/Materials:** The fuel canopy will be a steel, flat-roof structure open on all four sides. The roof will be supported by eight steel interior columns aligned with the fuel pumps. The canopy façade will be aluminum composite panel with ARCO signage.

The Convenience Store and Fast Food Restaurant would be housed in a single 4,956 square foot building. There will be two restrooms located in the building accessed by both functions. The building would have a flat roof with a parapet surround averaging four feet above the roof plane and would be accented with a standing-seam metal pitched roof façade along the sides facing Green Valley Road and Sophia Parkway. The Green Valley Road side would also be accented with two rough-sawn wood supported dormers over the entrances. The exterior walls would be cement stucco painted in earth tones with colored accents and cement stone corner towers and wainscot.

The carwash is a pre-fabricated unit made in Italy with a vision glass wall facing the parking lot, painted cement stucco along the eastern property line, and cement stone accents to match the adjacent building. The roof is an acrylic barrel-vault skylight. The surround trim is an aluminum composite panel to match the canopy graphics.

The trash enclosure would be constructed of concrete masonry units painted to match the adjacent building and would have a steel gate painted to match the enclosure.

The retaining wall along the southern end of the development is anticipated to be constructed of steel "H" piles drilled into the underlying bedrock. The steel piles will be in-filled with timber lagging stained in an earth tone. The retaining wall along the eastern boundary would be constructed of stacked cement modular units stained in an earth tone.

**Fire Protection:** Policy 5.7.1.1 requires that adequate emergency water supply, storage and conveyance facilities, and access for fire protection either are or would be provided concurrent with development. The El Dorado Hills Fire Department is recommending that the water system provide a minimum fire flow of 1,500 gallons per minute with a minimum residual pressure of 20 psi for a two-hour duration. Additionally, the Fire Department requires that the applicants install two fire hydrants.

Policy 6.2.3.2 requires that adequate access exists, or can be provided for emergency vehicles and private vehicles to access and evacuate the area. The Fire Department requires fire apparatus access to within 150 feet of all portions of the exterior walls of the building. Fire access is required to be capable of supporting fire apparatus weighing at least 40,000 pounds. The project, as designed, meets these requirements.

**Grading and Drainage:** All grading activities exceeding 250 cubic yards of graded material or grading completed for the purpose of supporting a structure must meet the provisions contained in the County of El Dorado - Grading, Erosion, and Sediment Control Ordinance. The development plans for the proposed buildings would also be required to implement the Uniform Building Code Seismic construction standards. Fills on the order of about 13 feet (maximum)

are proposed to raise the site above Sophia Parkway and to an elevation equal with Green Valley Road. Approximately 18,000 cubic yards of imported soil is proposed in order to bring the northern portion of the site to street level.

The post-project drainage would be handled by an underground vault system and drainage volumes would be required to be substantially the same pre and post project.

As conditioned for stream restoration, mitigated for compliance with California Water Quality Control Board standards, and with implementation of Best Management Practices during the grading permit process, no adverse increase in the overall runoff and flows are expected. The project would be required to conform to the El Dorado County Grading, Erosion Control and Sediment Ordinance.

Grading and Drainage are discussed in more detail in the Biological Resources, Geology and Soils, Hydrology and Water Quality, and Utilities and Service System sections of the proposed Mitigated Negative Declaration and Initial Study, Exhibit Q.

**Landscaping:** County Code requires the use of landscaping to buffer commercial parking areas from adjoining streets and as screening from residential land uses. As shown on the landscaping plan in Exhibit I, the project would include landscaping buffers along the perimeters of parking areas and property boundaries. The majority of the proposed plants are listed in the El Dorado County Drought Resistant Plant List. There are evergreen trees proposed along the east and south development area boundaries to buffer views of the project from the residents in those areas. The project biologist and landscape architect coordinated to include willow trees native to the riparian habitat along the southern retaining wall. Valley oaks, also native to this particular riparian area, are proposed to be the street trees for along Sophia Parkway and Green Valley Road.

**Land Use Compatibility:** Policy 2.2.5.21 directs that development projects shall be located and designed in a manner that avoids incompatibility with adjoining land uses. There are residences located southeast of the project that are higher in elevation than the project parcel that would have views of the project. Mitigation measures are included that require the preservation and enhancement of the southern approximately 1/3<sup>rd</sup> of the parcel beginning ten feet north of the creek. The existing trees, as well as the required cottonwood and valley oak trees in the restoration would partially buffer those views in the long run. The landscape plan includes evergreen trees along the proposed retaining wall to further buffer those views. The lights would be limited to a height of 16 feet and would be full cutoff fixtures that would prevent sky lighting and trespass horizontally off the parcel. Planning is recommending no signs be permitted on the south and east sides to further soften the commercial look from those residences. The noise analysis has demonstrated that the carwash-related noise would not exceed General Plan noise standards.

The building's architecture, materials, and colors would be consistent with those of other commercial businesses along Green Valley Road between the El Dorado/Sacramento county line and Salmon Falls Road. The design is consistent with the design of the buildings approved in the Green Valley Market Place (the Safeway Shopping Center). This Development Plan is allowing

for a service station, carwash, fast-food restaurant, and a convenience store. As conditioned and mitigated, there are no significant conflicts anticipated with any other existing or proposed commercial uses in the area. The project's potential impacts from noise, lighting, traffic and circulation have been analyzed. That is the purpose of this Planned Development request.

**Lighting:** Policy 2.8.1.1 directs that excess nighttime light and glare be limited from the parking area lighting, signage and buildings. The Site Lighting Photometric (Sheet ES1.2, Exhibit K), shows the project would include shielded wall lights, recessed canopy lights, and the pole lights would be 12-feet tall with a three-foot concrete base with full cutoff fixtures. The photometric analysis demonstrates that the project would not create significant amounts of light outside of the parcel boundaries. A condition of approval is recommended to assure that the lights are compliant with Section 17.14.170 of the Zoning Ordinance, be fully shielded pursuant to the Illumination Engineering Society of North America's (IESNA) full cut-off designation, and that the pole lights do not exceed 16 feet tall. As proposed, the lighting plan would be compliant with this Policy.

Lighting is discussed in more detail in the Aesthetics section of the proposed Mitigated Negative Declaration and Initial Study.

**Parking:** Zoning Ordinance Section 17.18.060 requires that the convenience store include one space per 300 square feet of enclosed gross retail sales floor area (2,773/300) or 9.3 spaces. The restaurant is required to have one space for three seats. The restaurant is proposed to have 49 seats so it requires 16.3 spaces. Of those 26 required spaces, two are be required to be accessible and one of those with van accessibility (Table 11B-6, 2010 California Building Code). Of those 26 spaces, 35 percent are permitted to be compact spaces (9). The applicants are proposing 16 standard spaces, eight compact spaces, and two accessible spaces (one of those with van accessibility) which Planning has determined would be adequate for the project.

The project would need to include provisions for bicycle parking on the site by providing bicycle spaces/racks, within the 100 feet each building (five percent of total parking spaces, and pursuant to Section 5.105.4.1 and 2 of the 1020 California Green Building Standards Code), at locations at the discretion of the applicants but convenient from adjoining streets and distributed proportionally. Because 26 total spaces are included, 5.2 bicycle parking spaces are required. The project plans include six spaces.

Section 17.18.080.C requires one 12 foot by 40 foot off-street loading space. The project includes that space, shown on the site plan in the northern portion of the site.

**Signage:** The C Zone District allows one 80 square-foot free-standing sign or two 50 square-foot signs. The applicants have proposed one monument sign and wall signs on the canopy, store and restaurant building, and car wash (Exhibits J-1 – J-3.) The total square footage of the monument sign is 79.9 square feet. The proposed height is 20 feet, 8.75 inches. The final location will be reviewed by DOT during the Building Permit process for any potential conflict with the required line-of-sight.



Wall signs are permitted by Zoning Ordinance Section 17.16.030, as long as the maximum allowable 20 percent of wall coverage is not exceeded. The wall signs, as well as the monument signs are shown in the colored elevation and site picture exhibits.

Planning has included recommended conditions of approval to reduce the height of the monument sign to 16 feet, consistent with the free standing pole sign for the gas station on the north side of Green Valley Road, approximately 500 feet to the east. (That sign is 15 ft, 6 inches tall and was originally approved by DR98-0017-S in 2004.) Additionally, Planning is recommending restricting signs proposed for the south and east-facing building walls because they are facing residential and commercial areas where there would be no traffic viewing the site.

**Sewage Disposal and Water:** The applicants propose to connect to existing water and sewer service from EID. The El Dorado Irrigation District Facility Improvement Letter dated December 7, 2012 reported there was adequate capacity in their existing facilities to serve the project, with District-approved extensions. This is discussed in more detail in the Utilities and Service Systems section of the proposed Mitigated Negative Declaration and Initial Study.

**Public Comments:** The El Dorado Hills Area Planning Advisory Committee reviewed the project to review this project. APAC voted 6 to 0 for conditional support of the project if their concerns about traffic, circulation, the wetland setback, and signs were addressed (Exhibit N). The Committee recommended the following:

- a. Prohibit left turns into project;
- b. Add an acceleration/deceleration lane on Green Valley Road;
- c. Provide additional mitigation to compensate for the reduced wetland setback;
- d. Revise traffic impact analysis;
- e. Modify on-site circulation to better accommodate drive-through;
- f. Reduce height of sign; and
- g. Design retaining wall to be aesthetically pleasing to residents to the southeast.

Left turns – Left turns will be prohibited and a raised median is required to prevent such turns (Condition 24).

Accel/Decel lane – The Transportation Division reviewed the need to construct an additional lane to serve the project and determined it is unnecessary. The speed of the traffic is controlled by the proximity to the signalized intersection, there are numerous other encroachments onto Green Valley Road without accel/decel lanes, and the volume of traffic does not warrant the lane. The only development that has an accel/decel lane on Green Valley is the Safeway east of Francisco Drive. Traffic speeds are typically higher there because it is further away from the signalized intersection to the east and has a much higher volume of traffic due to its size.

Wetland encroachment – APAC recommended that the car wash and restaurant be eliminated and the applicant is “trying to cram too much onto this site.” This is forcing the need to encroach into the wetland area. As discussed in the intermittent stream section of this report, the applicants have complied with the requirements of the Interim Guidelines for implementation for Policy 7.3.3.4 and the U.S. Army Corps of Engineers. Appropriate permits from the California

Department of Fish and Wildlife are also required as conditions. The project meets the development standards for parking and circulation.

Traffic analysis – The traffic analysis utilized standard protocols for developing a traffic study.

On-site circulation – APAC expressed concerns that queuing for the drive through could block access from Sophia Parkway. Staff reviewed the site plan and there is room for at least six vehicles form the pick-up window. There is an equal amount of space for the car wash. This appears to be sufficient based on a review of other drive-through services.

Sign height – APAC recommended that the sign be reduced in height to 12 feet. Staff is recommending 16 feet, which is a reduction from the original proposal of over 20 feet. The 16 foot height is consistent with the recently approved sign on the north side of the Green Valley Road as discussed in the sign section of this report.

Retaining wall – APAC expressed concerns about the visual impact of the retaining wall along the eastern boarder of the property. The final design of the wall has not been completed however the applicant indicated that the preliminary design is to utilize driven steel with timber. The outside of the wall is heavily landscaped (Exhibit I) which should provide adequate screening for the adjacent residential lots.

## **ENVIRONMENTAL REVIEW**

Staff has prepared an Initial Study (Exhibit Q) to determine if the project has a significant effect on the environment. Potentially significant effects of the project on the environment have been mitigated by recommended conditions that avoid or lessen the impacts to a point of insignificance; therefore a Mitigated Negative Declaration has been prepared and a Notice of Determination (NOD) will be filed. A \$50.00 filing fee for the NOD is required and the NOD must be filed within five working days from the project approval.

The filing of the NOD begins the statute of limitations time period for when litigation may be filed against the County's action on the project. If the NOD is filed the statute of limitations ends 30 days from its filing. If no NOD is filed, it ends 180 days from the date of final action by the County.

In accordance with California Fish and Game Code Section 711.4, the project is subject to a fee of \$2,156.25 after approval, but prior to the County filing the Notice of Determination on the project. This fee plus the \$50.00 filing fee, is to be submitted to Planning Services and must be made payable to El Dorado County. The \$2,156.25 is forwarded to the State Department of Fish and Wildlife and is used to help defray the cost of managing and protecting the State's fish and wildlife resources.

## SUPPORT INFORMATION

### Attachments to Staff Report:

Attachment 1 .....	Conditions of Approval
Attachment 2 .....	Findings
Exhibit A .....	Location Map
Exhibit B .....	Assessor's Parcel Map
Exhibit C .....	Parcel Map PM50-82
Exhibit D .....	General Plan Land Use Designations Map
Exhibit E .....	Zoning Designations Map
Exhibit F .....	Cover Sheet, Sheet T.1; April 12, 2013
Exhibit G .....	Site Plan, Sheet C2.0 of 6; April 12, 2013
Exhibits H-1 to H-5 .....	Building Elevations
Exhibit I .....	Landscape Planting Plan, Sheet L1 of 5; April 12, 2013
Exhibits J-1 to J-3 .....	Sign Site Plan, Sheets SNA.1 and SNA.2-April 12, 2013; Sheet SNA.3-dated December 6, 2012
Exhibit K .....	Site Lighting Photometric, Sheet ES1.2; April 12, 2013
Exhibit L .....	Trash Enclosure, Sheet TE.1; October 12, 2012
Exhibit M .....	Project Colors
Exhibit N .....	El Dorado Hills Area Planning Advisory Committee Letter; March 16, 2013, (four pages)
Exhibits O-1 to O-3 .....	Site Visit Photos
Exhibits P-1, P-2 .....	Aerial Photos
Exhibit Q .....	Proposed Mitigated Negative Declaration and Initial Study



124-301-46

POR. SECS. 21 & 28, T.10N., R.8E., M.D.M.

124:30

RECEIVED  
PLANNING DEPARTMENT

- \*PARCEL NOTES**  
All parcels on this page are Assessor's Block 1.
- 17 - PM 14/145/3
  - 18 - PM 14/129/A, 0.23 A
  - 19 - PM 14/129/B, 0.169 A
  - 20 - PM 14/129/C, 0.23 A
  - 22 - PM 10/1/1
  - 23 - PM 10/1/2
  - 26 - PPM 46/65/3
  - 27 - PPM 46/65/2
  - 31 - PM 6/65
  - 32 - PPM 6/65/B
  - 33 - PM 3/68/A
  - 34 - PM 14/128/1, 0.46 A
  - 35 - PPM 14/128/2, 0.408 A
  - 44 - PM 14/145/2, 0.32 A
  - 45 - PM 14/145/1, 0.19 A

Folsom Lake

Bk 110 Pg 44



Acreages Are Estimates

Adjacent Map Pages Shown in Gray Text  
Assessor Block Numbers Shown in Ellipses  
Assessor Parcel Numbers Shown in Circles

Rev. Dec. 1, 2010

Assessor's Map Bk. 124 - Pg. 30  
County of El Dorado, CA

THIS MAP IS NOT A SURVEY. It is prepared by the El Dorado Co. Assessor's office for assessment purposes only. Area calculations and characteristics are not guaranteed. Users should verify facts such as dimensions and acreage.

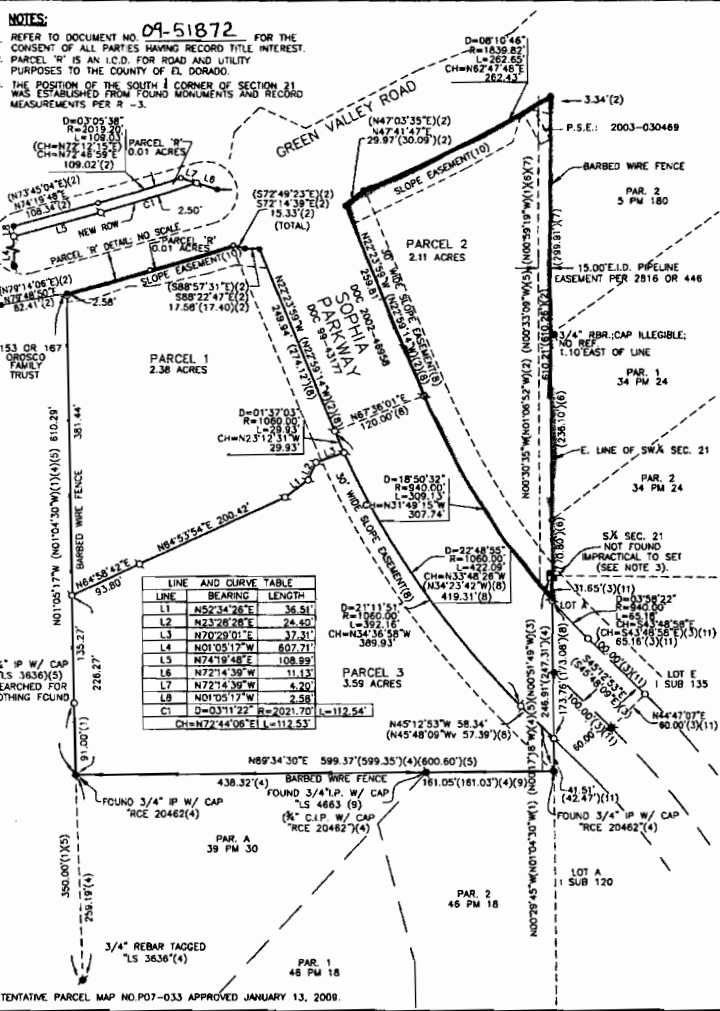
Exhibit B

S 12-0015/PD 12-0003

124-2011-44

78105

50182



### PARCEL MAP

A PORTION OF THE NW 1/4 OF SEC.28 AND THE SW 1/4 OF SEC.21  
T. 10 N., R. 8 E., M.D.M.  
COUNTY OF EL DORADO STATE OF CALIFORNIA  
PRECISION LAND SURVEYING, INC. SEPTEMBER 2009  
8850 AUBURN FOLSOM ROAD, GRANITE BAY, CA (916) 789-2005

**LEGEND**

- PUBLIC SERVICE EASEMENT
- FOUND 5/8" REBAR W/ ALUMINUM CAP AND PUNCH STAMPED EL DORADO Co. D.O.T. (2)
- ✱ FOUND COPPER WELD W/ CAP STAMPED LS 5854 (3)
- QUARTER CORNER AS NOTED
- FOUND MONUMENT AS NOTED
- DIMENSION POINT—NOTHING FOUND OR SET UNLESS OTHERWISE NOTED
- ⊕ SET "M" REBAR W/ PLASTIC CAP "LS 6376"

**REFERENCES**

- (1) 2008-0083815
- (2) 27 RS 39
- (3) THE PROMONTORY VL. 2 SD - 1 135
- (4) 39 PM 30
- (5) 4 RS 129
- (6) 34 PM 24
- (7) 5 PM 180
- (8) 2002-0046958
- (9) 46 PM 18
- (10) 2001-0077525-00
- (11) THE PROMONTORY VL. 1 SD - 1 120

**BASES OF BEARINGS:**  
THE MERIDIAN OF THIS SURVEY IS THE NORTH LINE OF PARCEL 'A' AS SHOWN ON THAT PARCEL MAP FILED IN BOOK 39 OF PARCEL MAPS AT PAGE 30, EL DORADO COUNTY OFFICIAL RECORDS, DETERMINED FROM MONUMENTS SHOWN HEREON AS FOUND.

**SURVEYOR'S STATEMENT**  
THIS MAP WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY IN CONFORMANCE WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE AT THE REQUEST OF R.S.C. ENGINEERING, INC. ON MARCH 26, 2009.  
I HEREBY STATE THAT THIS PARCEL MAP SUBSTANTIALLY CONFORMS TO THE APPROVED OR CONDITIONALLY APPROVED TENTATIVE MAP, IF ANY, ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.  
DATED: 09-08-09 Richard A. Marino  
RICHARD A. MARINO  
L.S. 6376 EXPIRES 12-31-10

**COUNTY SURVEYOR'S STATEMENT:**  
I HAVE EXAMINED THE MAP, THE SUBDIVISION AS SHOWN IS SUBSTANTIALLY THE SAME AS IT APPEARED ON THE TENTATIVE MAP, IF REQUIRED, AND ANY APPROVED ALTERATIONS THEREOF. ALL PROVISIONS OF CHAPTER 2 OF THE SUBDIVISION MAP ACT AND OF ANY LOCAL ORDINANCES APPLICABLE AT THE TIME OF APPROVAL OF THE TENTATIVE MAP, IF REQUIRED, HAVE BEEN COMPLIED WITH. I AM SATISFIED THE MAP IS TECHNICALLY CORRECT.  
GREEN VALLEY ROAD INCLUDING THE UNDERLYING FEE FOR ANY AND ALL PUBLIC PURPOSES AS SHOWN ON THE MAP AS PARCEL 'R' IS HEREBY ACCEPTED.  
THE PUBLIC UTILITIES EASEMENT AS SHOWN ON THE PARCEL MAP AS GREEN VALLEY ROAD, SHOWN AS PARCEL 'R', IS HEREBY ACCEPTED ON BEHALF OF THOSE PUBLIC ENTITIES THAT WILL PROVIDE SERVICES, SUBJECT TO THE PURVEYOR'S CONSTRUCTION STANDARDS.  
FURTHER, THE COUNTY SURVEYOR HEREBY REJECTS THE OFFERS OF DEDICATION TO THE COUNTY OF EL DORADO FOR OTHER EASEMENTS AS SHOWN ON THE MAP.  
DATED: Oct 15, 2009  
DANIEL S. RUSSELL  
DANIEL S. RUSSELL L.S.5017 LICENSE EXPIRES 12-31-09  
COUNTY SURVEYOR  
COUNTY OF EL DORADO, CALIFORNIA

**COUNTY RECORDER'S STATEMENT**  
I, WILLIAM E. SCHULTZ HEREBY CERTIFY THAT PLACER TITLE COMPANY PARCEL MAP GUARANTEE NO. 023-000310 WAS FILED WITH THIS OFFICE AND THAT THIS PARCEL MAP WAS ACCEPTED FOR RECORD AND FILED THIS 15 DAY OF OCTOBER, 2009, AT 10:19:28 AM IN BOOK 60 OF PARCEL MAPS AT PAGE 30, AT THE REQUEST OF CEMO FAMILY PROPERTIES, LLC  
DOCUMENT NO. 2009-51871  
William E. Schultz  
WILLIAM E. SCHULTZ  
COUNTY RECORDER, CLERK  
COUNTY OF EL DORADO, CALIFORNIA  
BY: Nathan W. Hill  
DEPUTY

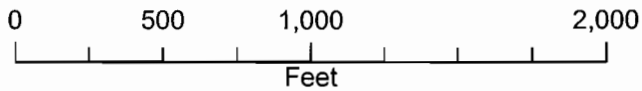
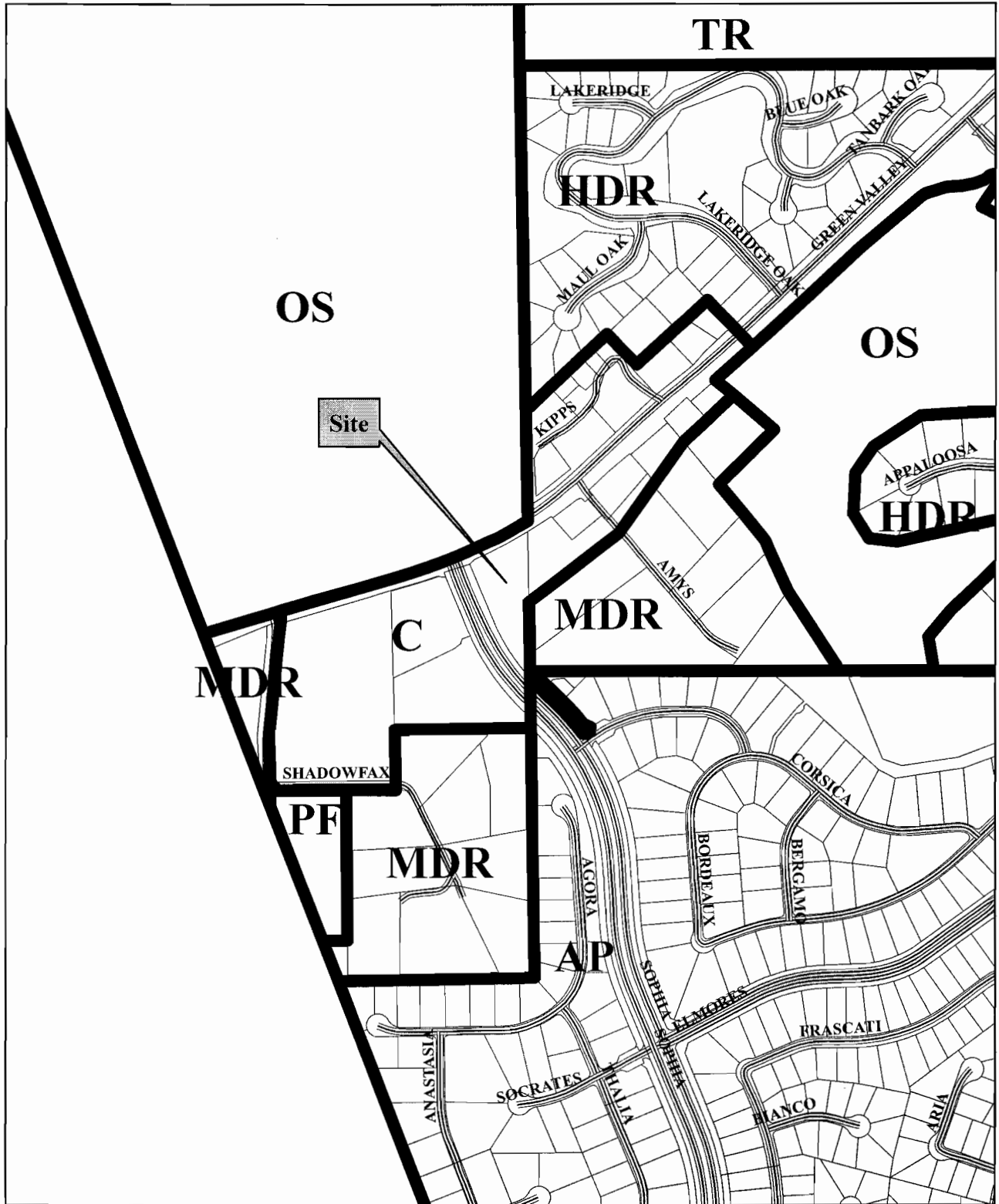
EXISTING A.P.N. 124-300-98 SHEET 1 OF 1

Exhibit C

50182

S 12-0015/PD 12-0003

# General Plan Land Use Designations

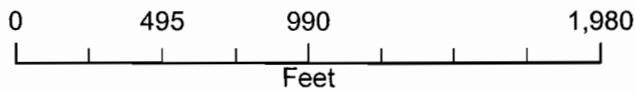
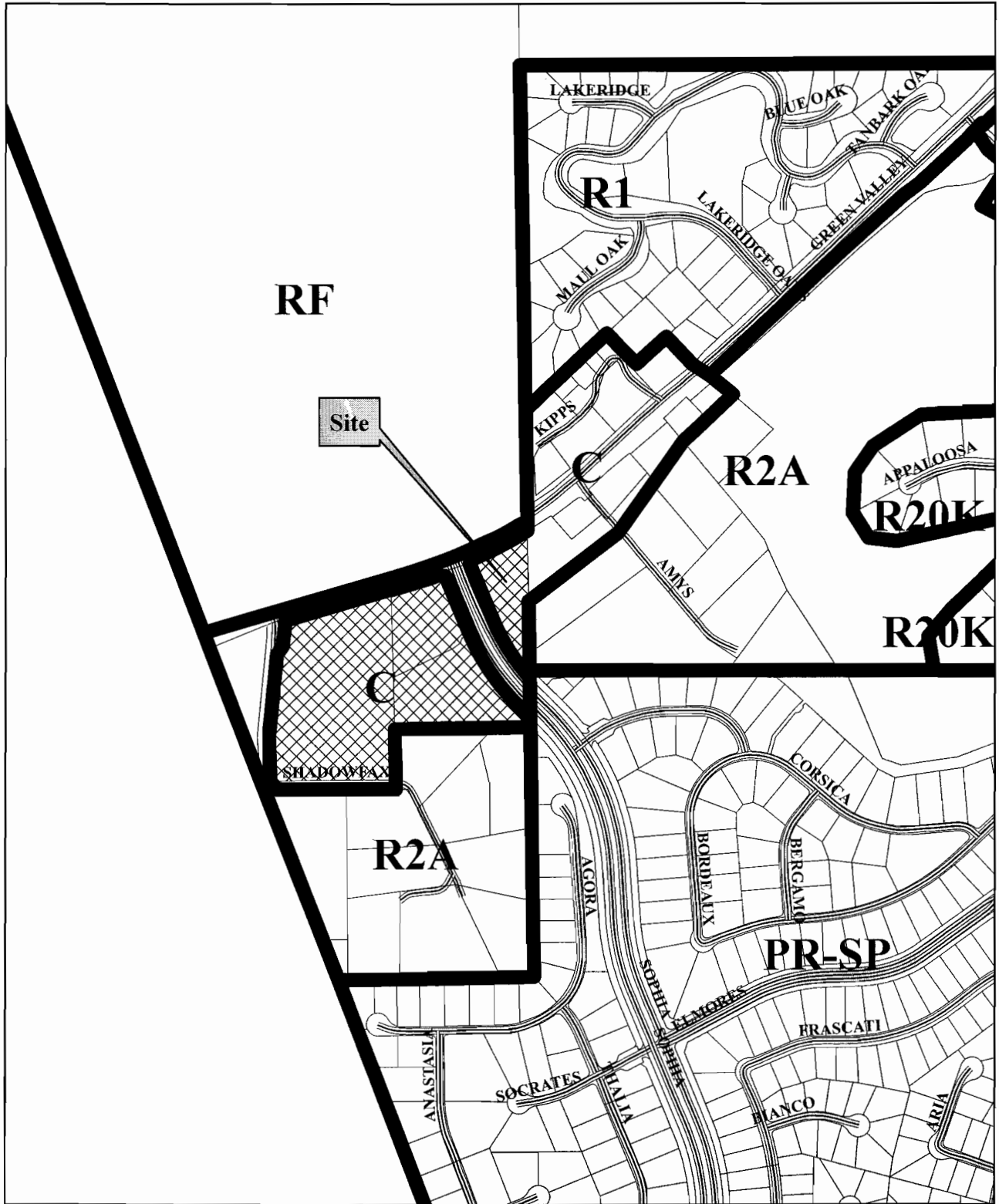


File Number PD12-0003

Exhibit D

STAFF REPORT  
13-1347 K 15 of 39

# Zoning Designations



 Planned Development Overlay

File Number PD12-0003

Exhibit E

STAFF REPORT  
13-1347 K 16 of 39









1 NORTH ELEVATION  
1/8"=1'-0"



2 WEST ELEVATION  
1/4"=1'-0"

**EXTERIOR FINISHES:**

- FC-1 ALUMINUM SIDING
- FC-2 BRICK
- FC-3 MANUFACTURED STONE
- FC-4 STAMPING GRANITE
- FC-5 BRICK

**EXTERIOR PAINTS:**

- CO-1 COLOR: BRICK
- CO-2 COLOR: BRICK
- CO-3 COLOR: BRICK
- CO-4 COLOR: BRICK
- CO-5 COLOR: BRICK
- CO-6 COLOR: BRICK
- CO-7 COLOR: BRICK
- CO-8 COLOR: BRICK
- CO-9 COLOR: BRICK
- CO-10 COLOR: BRICK
- CO-11 COLOR: BRICK
- CO-12 COLOR: BRICK
- CO-13 COLOR: BRICK
- CO-14 COLOR: BRICK
- CO-15 COLOR: BRICK
- CO-16 COLOR: BRICK
- CO-17 COLOR: BRICK
- CO-18 COLOR: BRICK
- CO-19 COLOR: BRICK
- CO-20 COLOR: BRICK

**GENERAL NOTES:**

- 1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE BUILDING CODES AND SPECIFICATIONS.
- 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
- 3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.
- 4. THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES AND STRUCTURES.
- 5. THE CONTRACTOR SHALL MAINTAIN THE SITE IN A SAFE AND SOUND CONDITION THROUGHOUT THE PROJECT.
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES.
- 7. THE CONTRACTOR SHALL MAINTAIN THE SITE IN A SAFE AND SOUND CONDITION THROUGHOUT THE PROJECT.
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES.
- 9. THE CONTRACTOR SHALL MAINTAIN THE SITE IN A SAFE AND SOUND CONDITION THROUGHOUT THE PROJECT.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES.

Preliminary Not For Construction

Exhibit H-1





THIS SHEET IS FOR INFORMATION ONLY

ARCO NY  
2908 AVE  
NY 11750  
FUEL CANOPY 11' x 11' x 7' 6" AND 1,900 SF G.S.R.

GREEN VALLEY ROAD  
GREEN VALLEY, NY

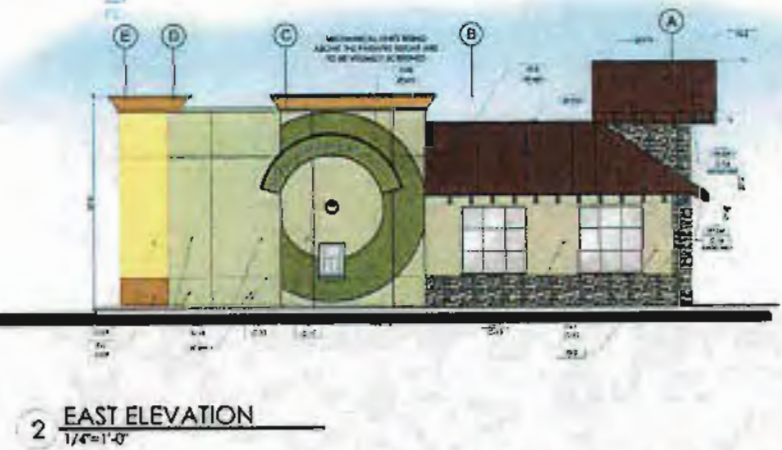
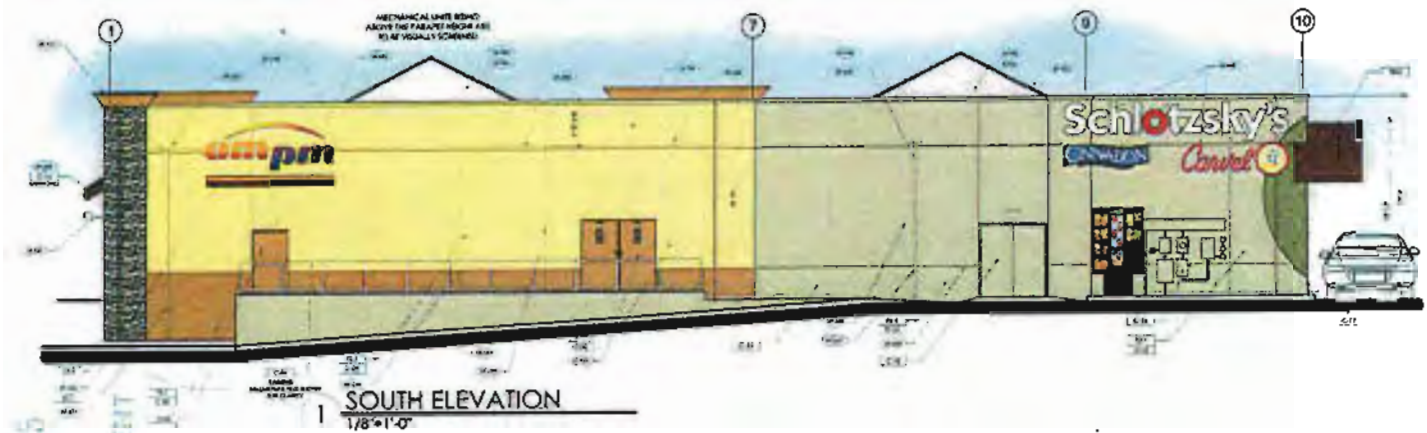
FACILITY # T10

VLD 15483

CONTRACT # 15483

A2.1

Exhibit H-2



- EXTERIOR FINISHES:**
- E-11 ALUMINUM ACH PANEL
  - E-12 LIGHT TANNED WOOD GRAIN (EXCEPT NORTH SIDE) WOOD
  - E-13 MANUFACTURED LIGHT WOOD
  - E-14 METAL COMPOSITION ROOF (SEE NOTES)
  - E-15 DIVIDED GLASS METAL ROOF WITH 1/2\"/>
- EXTERIOR PAINTS:**
- P-01 COLOR: SP. HAZEL, 100% 100% BY ADDING 10% WHITE TINT
  - P-02 COLOR: SP. GARDEN, 100% 100% VALLEY
  - P-03 COLOR: SP. HAZEL, DENVER, 100% 100%
  - P-04 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-05 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-06 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-07 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-08 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-09 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-10 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-11 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-12 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-13 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-14 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-15 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-16 COLOR: SP. HAZEL, 100% 100% WOOD
  - P-17 COLOR: SP. HAZEL, 100% 100% WOOD
- GENERAL NOTES:**
1. SPECIAL LOCATIONS IN HARD TO REACH SPACES ARE TO BE PAINTED AS NOTED.
  2. FINISHES ARE TO BE ACCORDING TO SCHEDULE.
- KEYED NOTES:**
1. SP. HAZEL, 100% 100% WOOD
  2. SP. HAZEL, 100% 100% WOOD
  3. SP. HAZEL, 100% 100% WOOD
  4. SP. HAZEL, 100% 100% WOOD
  5. SP. HAZEL, 100% 100% WOOD
  6. SP. HAZEL, 100% 100% WOOD
  7. SP. HAZEL, 100% 100% WOOD
  8. SP. HAZEL, 100% 100% WOOD
  9. SP. HAZEL, 100% 100% WOOD
  10. SP. HAZEL, 100% 100% WOOD
  11. SP. HAZEL, 100% 100% WOOD
  12. SP. HAZEL, 100% 100% WOOD
  13. SP. HAZEL, 100% 100% WOOD
  14. SP. HAZEL, 100% 100% WOOD
  15. SP. HAZEL, 100% 100% WOOD
  16. SP. HAZEL, 100% 100% WOOD
  17. SP. HAZEL, 100% 100% WOOD
  18. SP. HAZEL, 100% 100% WOOD
  19. SP. HAZEL, 100% 100% WOOD
  20. SP. HAZEL, 100% 100% WOOD

Preliminary Not For Construction

bp

ARCO

BARGHAUSEN

THIS SHEET IS FOR INFORMATION ONLY

ARCO INTL  
2880 HWY 177  
PUE. CANOPY 1/4\"/>

GREEN VALLEY ROAD  
1/4\"/>

FACILITY # 1780

DATE: 12/15/10  
DRAWN BY: J. BROWN  
CHECKED BY: J. BROWN  
SCALE: 1/8\"/>

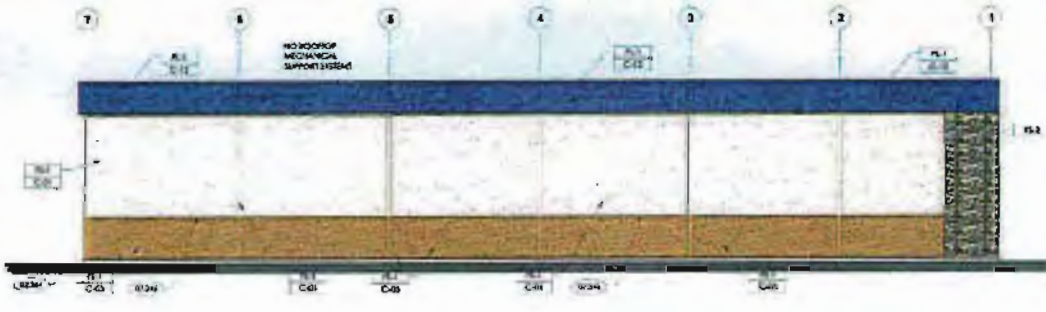
13-1347 K 20 of 39

A2.2

13 FEB -6 PM 2:45  
 RECEIVED  
 PLANNING DEPARTMENT



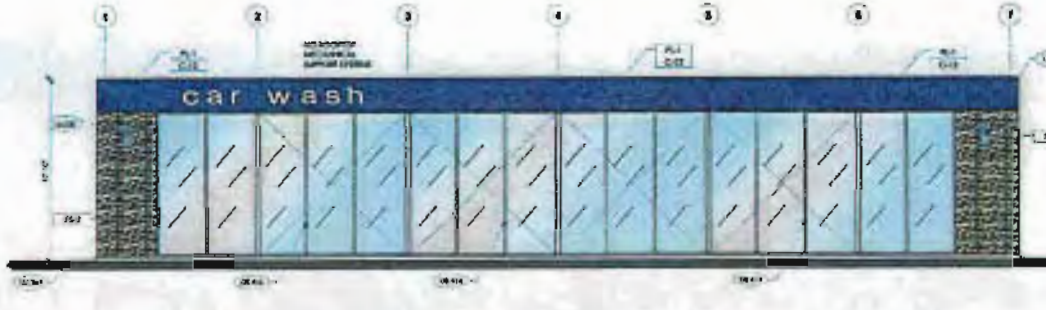
1 NORTH ELEVATION  
 1/4"=1'-0"



2 EAST ELEVATION  
 1/4"=1'-0"



3 SOUTH ELEVATION  
 1/4"=1'-0"



4 WEST ELEVATION  
 1/4"=1'-0"

**GENERAL NOTES:**  
 A. ALL DIMENSIONS UNLESS OTHERWISE NOTED ARE TO FACE UNLESS NOTED OTHERWISE.  
 B. FINISHES ARE TO BE IN ACCORDANCE WITH THE SPECIFICATIONS.  
**KEYED NOTES:**  
 10.24 GRADE SHALL BE PER PLAN  
 10.25 1/2" DIA. RAINFALL GUTTERS SHALL BE INSTALLED PER PLAN  
 10.26 2" DIA. DOWNSPOUTS SHALL BE INSTALLED PER PLAN  
 10.27 1/2" DIA. DOWNSPOUTS SHALL BE INSTALLED PER PLAN  
 10.28 1/2" DIA. DOWNSPOUTS SHALL BE INSTALLED PER PLAN  
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 10.99 1/2" DIA. DOWNSPOUTS SHALL BE INSTALLED PER PLAN  
 11.00 1/2" DIA. DOWNSPOUTS SHALL BE INSTALLED PER PLAN

**EXTERIOR FINISHES:**  
 F-1 ALUMINUM SIGN PANEL  
 F-2 1/2" DIA. DOWNSPOUT  
 F-3 1/2" DIA. DOWNSPOUT  
 F-4 1/2" DIA. DOWNSPOUT  
 F-5 1/2" DIA. DOWNSPOUT  
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 F-98 1/2" DIA. DOWNSPOUT  
 F-99 1/2" DIA. DOWNSPOUT  
 F-100 1/2" DIA. DOWNSPOUT

**bp**

**ARCO**

**BARGHAUSEN**  
 ARCHITECTS  
 1000 S. GARDEN ST.  
 SUITE 100  
 GREEN VALLEY, CA 95026  
 TEL: (925) 433-1100  
 FAX: (925) 433-1101  
 WWW.BARGHAUSEN.COM

**THIS SHEET IS FOR INFORMATION ONLY**

**ARCO HTI**  
 2500 AMT  
 11175 FM CA  
 FUEL CANOPY W/  
 AND 1,996 SF G.S.A.

**GREEN VALLEY ROAD**  
 6 SOUTH AVENUE  
 GREEN VALLEY, CA

**FACILITY # 780**

**DATE:** 05-05-06  
**SCALE:** 1/4"=1'-0"  
**PROJECT:** CW2.1

**CADSWAN**  
 EXTERIOR  
 ELEVATIONS

**CW2.1**

Exhibit H-3

Preliminary Not For Construction



1 NORTH ELEVATION (ALONG GREEN VALLEY ROAD)  
1/4"=1'-0"



2 WEST ELEVATION (ALONG SOPHIA PARWAY)  
1/4"=1'-0"



3 EAST ELEVATION  
1/4"=1'-0"



4 SOUTH ELEVATION  
1/4"=1'-0"

Preliminary Not For Construction

bp

ARCO  
www.arco.com

BARGHAUSEN  
PLANNING ENGINEERS

DATE: 08/04/05  
REV: 04/05/05  
PROJECT: 13-1347 K  
SHEET: H-4

THIS SHEET IS FOR INFORMATION ONLY

ARCO #71  
2900 SQ. FT.  
W/ 17'x65' CANOPY  
TOTAL CANOPY W/ 6 BAYS  
AND 1,000 SF G.S.R.

GREEN VALLEY ROAD  
@ BIRDA FARM  
CROSSWALK OF

FACILITY # TBD

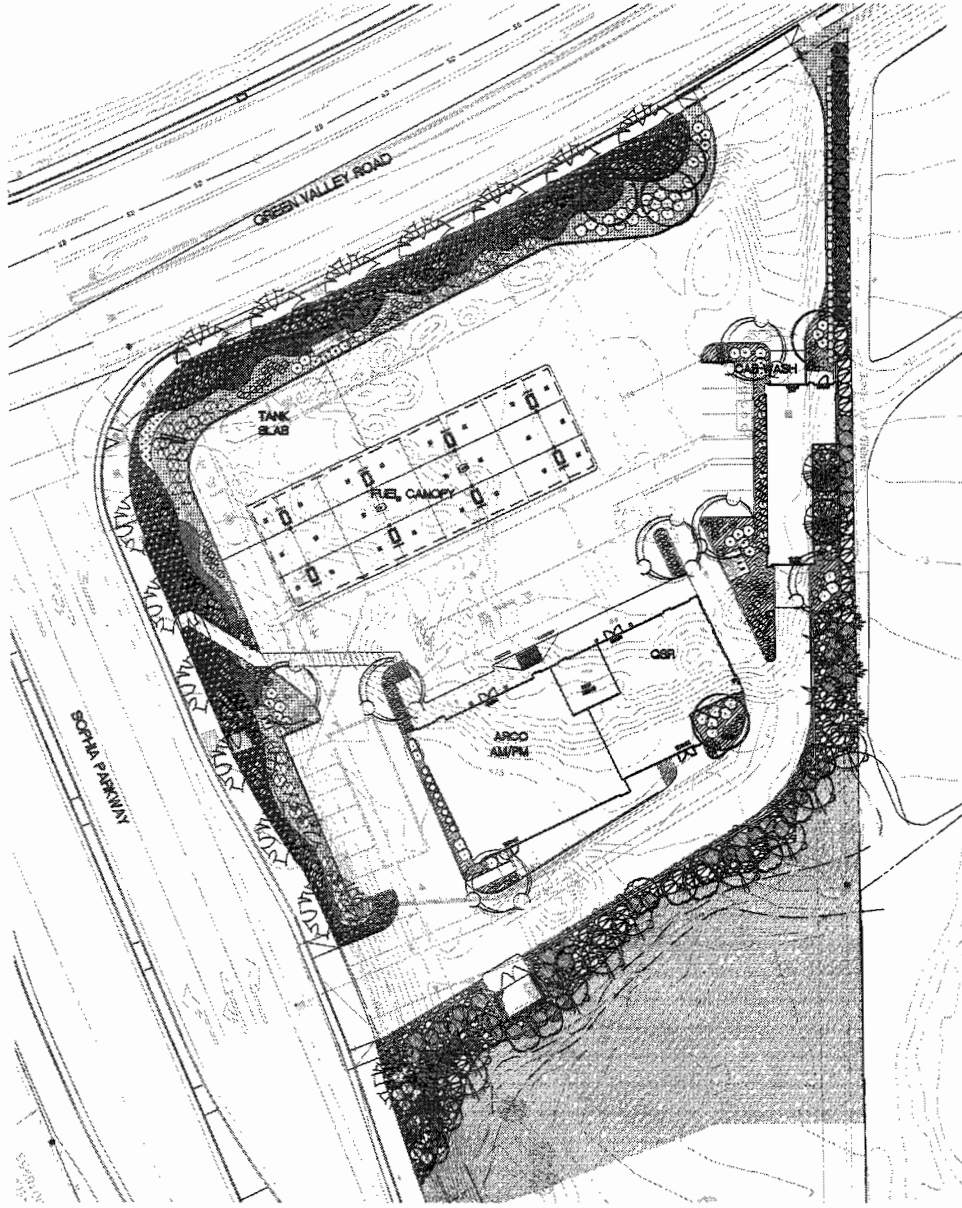
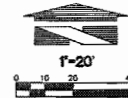
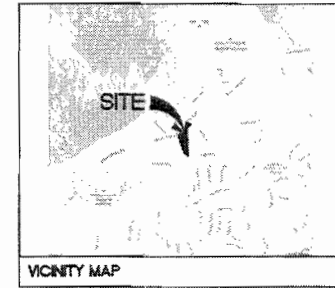
15593

CANOPY COLOR ELEVATIONS

RECORDED  
PLANNING DEPARTMENT  
13 FEB 05 PM 2:05



# ARCO AM/PM - ELDORADO HILLS, CALIFORNIA LANDSCAPE PLANTING PLAN



### LANDSCAPE PLANT MATERIAL LEGEND

SYMBOL	BOTANICAL / COMMON NAME	SIZE	COLLECTION	SPACING	QUANTITY	REMARKS
<b>TREES:</b>						
	CEDRUS DEODORA / DEODAR CEDAR	5 GALLON	AS SHOWN	5		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN, 1A-CUT LEADER
	LAGERSTROEMIA INDICA 'MARDI GRAS' REV. / LAGERSTROEMIA INDICA 'MARDI GRAS' REV.	10 GALLON	AS SHOWN	5		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	PHILADELPHUS 'FRODO BAGGINS' / PHILADELPHUS 'FRODO BAGGINS'	15 GALLON	AS SHOWN	2		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN, 1A-CUT LEADER
	QUERCUS ILEX / HOLLY OAK	5 GALLON	AS SHOWN	15		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	QUERCUS ILEX / HOLLY OAK	18 GALLON	AS SHOWN	7		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	QUERCUS LOBATA / VALLEY OAK (CALIF. TREE)	24" BOX	30' C.C.	14		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	SALIX LASIOLEPIS / ARROYO WILLOW	5 GALLON	30' C.C.	18		STAGE & CUT ONE GROWING SEASON; NURSERY GROWN, 1A-CUT LEADER
<b>SHRUBS:</b>						
	GRAMMATOCYPRIPIS ACULEATA 'DARK FOUNTAIN' / DARK FOUNTAIN RICE GRASS	1 GALLON	2' O.C.	45 REEDS		FILL
	CHOISYA MONNINA 'LUTEA' / CHOISYA MONNINA 'LUTEA'	5 GALLON	3' O.C.	8		FILL AND BUSH
	CORDYLINE 'TROPIC BLUE' / CORDYLINE 'TROPIC BLUE'	5 GALLON	5' O.C.	25		FILL AND BUSH
	CORDYLINE 'TROPIC BLUE' / CORDYLINE 'TROPIC BLUE'	1 GALLON	2.5' O.C.	186		FILL AND BUSH
	CEDRUS LIBANI 'SERRANO' / CEDRUS LIBANI 'SERRANO'	5 GALLON	4' O.C.	14		FILL AND BUSH
	CEDRUS LIBANI 'SERRANO' / CEDRUS LIBANI 'SERRANO'	1 GALLON	2' O.C.	87		FILL
	ESCALLONIA 'NEWPORT DAWN' / DWARF ESCALLONIA	5 GALLON	4' O.C.	24		FILL AND BUSH
	FREMONTODENDRON 'CALIFORNIA GLORY' / CALIFORNIA GLORY FLAMEO BUSH	5 GALLON	5' O.C.	35		FILL AND BUSH
	METROSIDEROS 'HAWAII' / METROSIDEROS 'HAWAII'	5 GALLON	5' O.C.	44		FILL AND BUSH
	NERIUM 'NERIUM' / NERIUM 'NERIUM'	5 GALLON	4' O.C.	5		3 CAMES BARRAM, FILL AND BUSH
	NERIUM 'NERIUM' / NERIUM 'NERIUM'	5 GALLON	3' O.C.	21		3 CAMES BARRAM, FILL AND BUSH
	PHILADELPHUS 'FRODO BAGGINS' / 'FRODO BAGGINS' SHRUB	5 GALLON	4' O.C.	13		FILL AND BUSH
<b>GROUNDCOVERS:</b>						
	ARUNDINACEAE 'PONY FEET' / PONY FEET GRASS	PLANT	12" C.C.	45 REEDS		HOLD 12" FROM BORDERS, SHRUBS AND TREES
	BACCARIS PILULARIS 'TWIN PEAKS' / TWIN PEAKS SHRUB	PLANT	12" C.C.	45 REEDS		HOLD 18" FROM BORDERS, SHRUBS AND TREES
	FRAGARIA CHANDLERI / BEACH STRAWBERRY	PLANT	12" C.C.	45 REEDS		HOLD 12" FROM BORDERS, SHRUBS AND TREES
	DATURA METEL 'YELLOW' / YELLOW TRUMPET CREEPER	PLANT	12" C.C.	45 REEDS		HOLD 12" FROM BORDERS, SHRUBS AND TREES
	ROSA 'RED FLOWER CARPET' / RED CARPET ROSE GROUNDCOVER	1 GALLON	24" C.C.	45 REEDS		HOLD 12" FROM BORDERS, SHRUBS AND TREES
	MULCH ONLY AREA					CNEP
	LINE BETWEEN GROUNDCOVER TYPES					
	EXISTING VEGETATION					TO REMAIN, SHC, AND PROTECT

**bp**

**ARCO**  
SHEET OFF PROJECT, LLC

**BARGHAUSEN**  
CONSULTING ENGINEERS, INC.

18215 72ND AVENUE SOUTH  
MONT, WA 98033  
(425)291-0223  
(425)291-0223 FAX

CIVIL ENGINEERING, LAND PLANNING,  
SUSTAINABLE ENVIRONMENTAL SERVICES

NO DATE REVISION DESCRIPTION

THIS SHEET IS FOR INFORMATION ONLY

ARCO NTI  
2900 sqm (pm)  
w/ 17'x55' CLAR WASH  
FUEL CANOPY w/ 8 MPD'S  
AND 1,900 SF Q.S.R.

GREEN VALLEY ROAD  
ELDORADO HILLS, CA

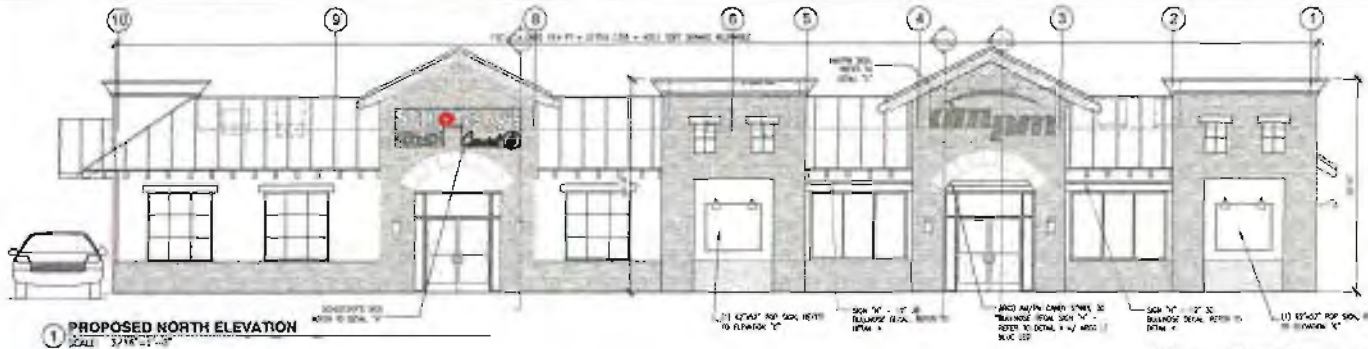
FACILITY # TBD

LANDSCAPE PLANTING PLAN

L1 OF 5

Exhibit I





1 PROPOSED NORTH ELEVATION  
SCALE: 3/4" = 1'-0"



4 AM/PM DECAL ON ACM BACKING  
SCALE: N/A



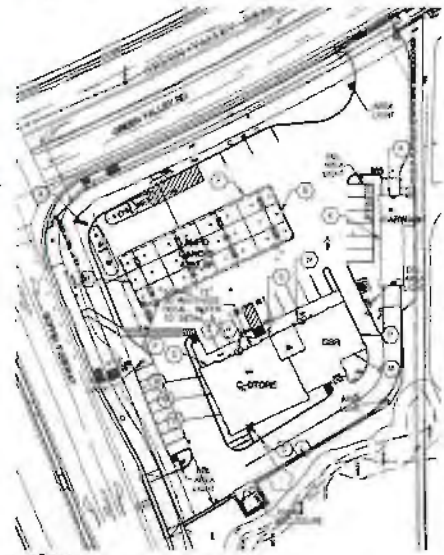
5 CANOPY DECAL ON ACM FASCIA w/LED  
SCALE: N/A



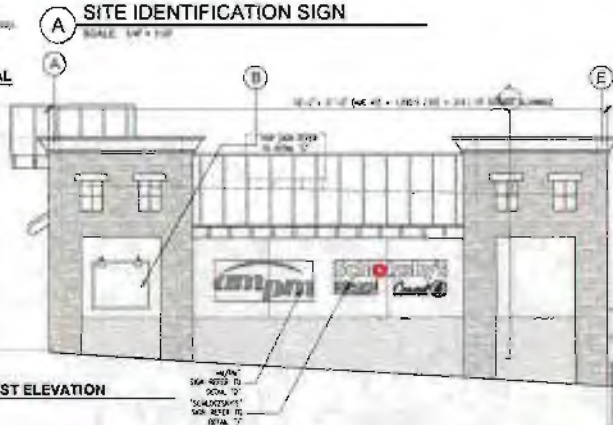
6 CANOPY SPARK 7.1 S.F. LOGO w/ DECAL  
SCALE: N/A



3 POP SIGN - 22.3 S.F.  
SCALE: N/A



SIGNAGE SITE PLAN  
SCALE: N.T.S.



2 PROPOSED WEST ELEVATION  
SCALE: 3/4" = 1'-0"

**SIGNAGE COMPANIES**  
MEMBERSHIP FEE - \$1,000.00  
ONE (1) SIGN NOT EXCEEDING 40 SQFT OF ANY DISPLAY SURFACE

TYPE	AREA	HEIGHT	DEPTH	TYPE	AREA	HEIGHT	DEPTH
STORAGE TOWER	ALLOWED						
MAIN BRANCH							
A-STORE	NORTH ELEVATION (PRIMARY)	454.1 SQFT					
	WEST ELEVATION	219.1 SQFT					
	SOUTH ELEVATION	432.8 SQFT					
	EAST ELEVATION	186.8 SQFT					
C-STORE	NORTH ELEVATION (PRIMARY)	125.9 SQFT					
FAST FOOD	NORTH ELEVATION	36.9 SQFT					
CANOPY	NORTH ELEVATION	28.8 SQFT					
		181.3 SQFT					
C-STORE	WEST ELEVATION	66.2 SQFT					
FAST FOOD	WEST ELEVATION	34.2 SQFT					
CANOPY	WEST ELEVATION	27.2 SQFT					
		164.3 SQFT					
C-STORE	SOUTH ELEVATION	65.5 SQFT					
FAST FOOD	SOUTH ELEVATION	12.1 SQFT					
CANOPY	SOUTH ELEVATION	18.8 SQFT					
		97.4 SQFT					
FAST FOOD	EAST ELEVATION	14.4 SQFT					
CANOPY	EAST ELEVATION	7.1 SQFT					
		21.2 SQFT					
CARWASH							
CARWASH	NORTH ELEVATION (PRIMARY)	12.5 SQFT					
CARWASH	WEST ELEVATION (PRIMARY)	12.6 SQFT					
		25.2 SQFT					
WORKING	SITE RI SIGN	180.0 SQFT					
		1,274.7 SQFT					

**SITE SIGNAGE TABLE**

NO.	SIGN	QTY	AREA
1	MAIN 18' SIGN/POLE SIGN (TIME TABLE)	1	138.8 sf
2	CANOPY SIGN (PRIMARY) (11' W/ 40')	2	144.2 sf
3	20' AM/PM CHANNEL LETTERS (33.6 sf ea.)	2	672.0 sf
4	CANOPY SIGN (SECONDARY) + LETTERS (16.4 W/ 40')	2	33.0 sf
5	POP SIGN (12' X 4')	3	65.0 sf
6	1'-0" LED ENTRY COLOR SIGN (9.0 ea.)	3	27.7 sf
7	SIGNAGE'S SIGN (10' X 10') (31.2 ea.)	3	93.2 sf
8	CARWASH SIGN (12' X 6')	2	24.0 sf
9	COLOR SIGN	2	11.5 sf
10	SIGNAGE'S SIGN (16' X 12') (12.1 ea.)	1	123.1 sf
11	SIGNAGE'S SIGN (10' X 10') (31.2 ea.)	1	31.2 sf
12	LED SIGN (10' X 10') (14.1 ea.)	1	14.1 sf
13	17' AM/PM CHANNEL LETTERS (42.8 sf ea.)	1	42.8 sf
TOTAL			599.4 sf



7 20' AM/PM CHANNEL LETTERS  
SCALE: N/A



GREEN VALLEY ROAD  
GREEN VALLEY  
GREEN VALLEY, CA  
FACILITY # 780  
15088

THIS SHEET IS FOR INFORMATION ONLY

ARCO NTI  
2800 am/pm  
w/ 17x48' CAR V  
FUEL CANOPY w/ 8  
AND 1,800 SF Q.S.

SIGN SITE PLAN AND ELEVATIONS

SNA.1

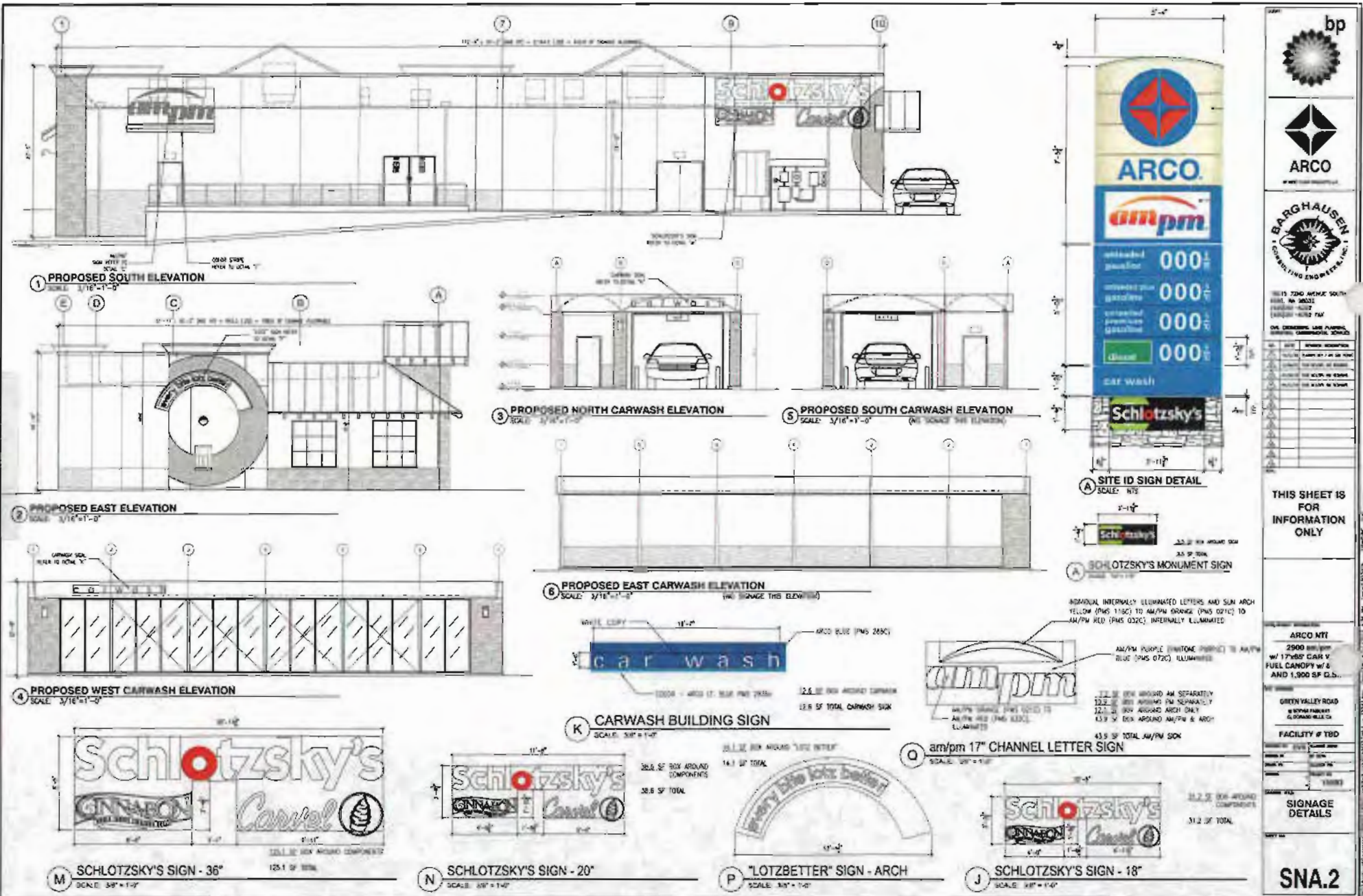
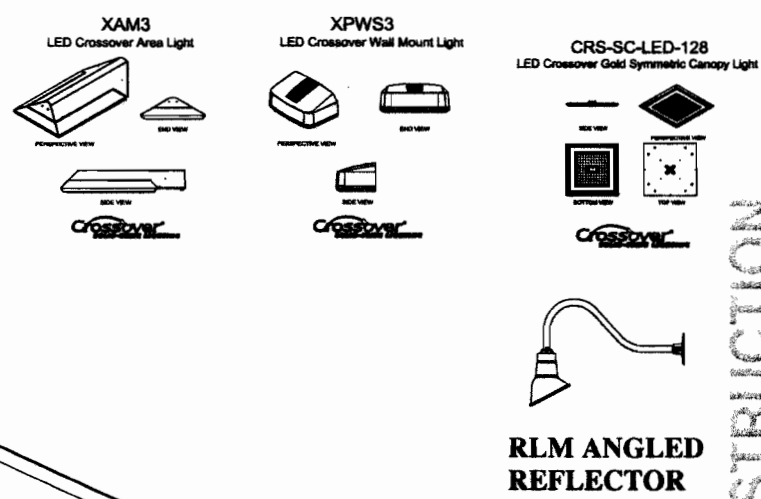
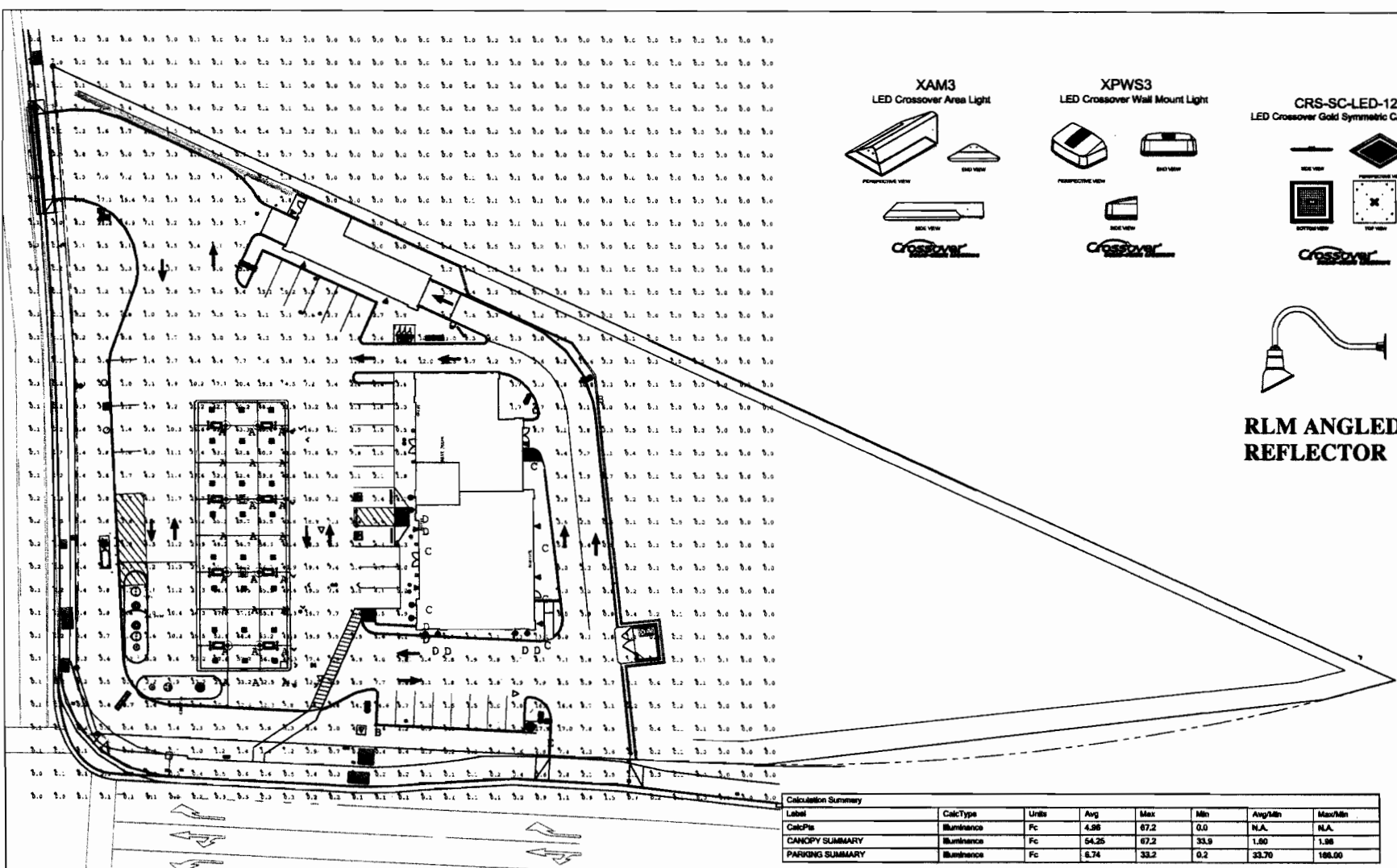




Exhibit K



NOT FOR CONSTRUCTION

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
CalcPs	Illuminance	Fc	4.96	67.2	0.0	N.A.	N.A.
CANOPY SUMMARY	Illuminance	Fc	54.25	67.2	33.9	1.90	1.98
PARKING SUMMARY	Illuminance	Fc	6.74	33.2	0.2	33.70	168.00

Symbol	Qty	Label	Arrangement	Description	LLF	Lumens/Lamp	Air. Lum. Lumens	Air. Watts
[Symbol]	24	A	SINGLE	CRS-SC-LED-128-SS-CW-UE	1.000	N.A.	16374	145
[Symbol]	2	B	SINGLE	XAM3-FT-LED-119-450-CW-UE-S-12 POLE+3' BASE	1.000	N.A.	12343	184
[Symbol]	9	C	SINGLE	XPWS3-FT-LED-48-350-CW-UE	1.000	N.A.	4024	52.8
[Symbol]	8	D	SINGLE	AD-150-16-CW-LED-UE-GWT-DO	1.000	N.A.	700	10
[Symbol]	2	E	2 @ 90 DEGREES	XAM3-FT-LED-119-450-CW-UE-090-12 POLE+ 3' BASE	1.000	N.A.	24686	368
[Symbol]	2			XAM3-FT-LED-119-450-CW-UE-090-12 POLE+ 3' BASE	1.000	N.A.	20762	368

Based on the information provided, all dimensions and luminaire locations shown represent estimated conditions. The engineer will conduct a site visit to verify the accuracy of the information provided. Actual conditions may vary from those shown. The engineer will be notified of any discrepancies. The engineer will be notified of any discrepancies. The engineer will be notified of any discrepancies.

Preliminary Not For Construction

**bp**

**ARCO**  
POWER CORP. FORT BELLEVILLE, ILL.

**BARGHAUSEN CONSULTING ENGINEERS**  
18215 72ND AVENUE  
ARNDT, MN 55305  
(435)251-4322  
(435)251-5782 FAX  
OIL, CHEMICAL, LIQUID PIPING, GENERAL, INDUSTRIAL, MARINE

NO.	DATE	REVISION DESCRIPTION
1	11/14/12	ISSUED FOR PERMITS
2	11/14/12	ISSUED FOR PERMITS
3	11/14/12	ISSUED FOR PERMITS
4	11/14/12	ISSUED FOR PERMITS
5	11/14/12	ISSUED FOR PERMITS
6	11/14/12	ISSUED FOR PERMITS
7	11/14/12	ISSUED FOR PERMITS
8	11/14/12	ISSUED FOR PERMITS
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24	11/14/12	ISSUED FOR PERMITS
25	11/14/12	ISSUED FOR PERMITS
26	11/14/12	ISSUED FOR PERMITS
27	11/14/12	ISSUED FOR PERMITS
28	11/14/12	ISSUED FOR PERMITS
29	11/14/12	ISSUED FOR PERMITS
30	11/14/12	ISSUED FOR PERMITS

**THIS SHEET IS FOR INFORMATION ONLY**

**ARCO MTI**  
2800 Highway 17  
W. FUEL CANOPY w/ 8' AND 1,800 SF G.S.J.

**GREEN VALLEY ROAD & 180TH STREET**  
BLOOMING HILLS, CA  
**FACILITY @ TBO**

DESIGNED BY: EVR  
CHECKED BY: EVR  
DATE: 11/14/12  
SCALE: AS SHOWN  
SHEET NO: 15693

**INDUSTRIAL LIGHTING PROPOSAL FOR**  
ARCO GREEN VALLEY & 180TH ST. BLOOMING HILLS, CA

PROJECT NO: LO-113388-3  
DATE: 11/14/12  
SHEET 1 OF 1

**ES1.2**



Exhibit M



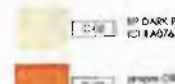
F5-2  
COLOR "DESERTADO  
STONE - SANDY PEAKS"



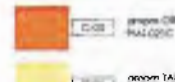
F5-3  
STANDING SEAM METAL ROOF  
MAG. SHIPLOCK  
COLOR: HUNTER RED



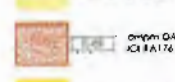
BP PEARL  
ICI #A0083 "INDIAN LEGEND"



BP DARK PEARL  
ICI #A0767 "DESERT VALLEY"



BP ORANGE  
ICI #A0767 "DESERT VALLEY"



BP TAN  
ICI #A0717 "INDIAN COURT"



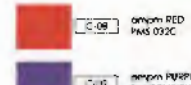
BP DARK TAN  
ICI #A1761 "SANDORKEETIAN"



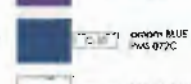
BP YELLOW  
ICI #A1162



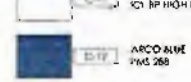
BP WARM GREY  
ICI #A1840 "GREY MOUNTAIN"



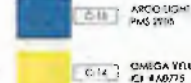
BP RED  
ICI #A0322



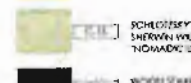
BP PURPLE  
PANTONE PURPLE



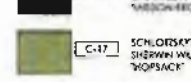
BP BLUE  
ICI #A0720



BP HIGH HIDING WHITE  
ICI #P1104 HIGH HIDING WHITE



ARCO BLUE  
ICI #A0720



ARCO LIGHT BLUE  
ICI #A0716



OMEGA YELLOW  
ICI #A0775



SCHLOTEK'S  
SHERWIN WILLIAMS #SW6107  
"INDIAN DESERT"



WOOD'S SHERWIN  
WILLIAMS #SW6107  
"INDIAN DESERT"



SCHLOTEK'S  
SHERWIN WILLIAMS #SW6107  
"INDIAN DESERT"



GREEN VALLEY ROAD  
@ SOPHA PARKWAY  
EL DORADO HILLS, CA

FACILITY # TBD

ARCO NTI  
2900 gal/pm  
w/ 17'x65' CAR WASH  
FUEL CANOPY w/ 8 MPD's  
AND 1,900 SF Q.S.R.



1825 2ND AVENUE SUITE  
400, WASH DC  
20001-4000  
202-555-1000  
202-555-1000 FAX  
THE ENGINEERING LAW FIRM  
OF ARCHITECTURAL DESIGN

RECEIVED  
PLANNING DEPARTMENT  
FEB-6 PM 2:45



13 MAR 16 PM 1:50

RECEIVED  
PLANNING DEPARTMENT

**El Dorado Hills Area Planning Advisory Committee**

1021 Harvard Way  
El Dorado Hills, CA 95762

**2013 Board Chair**

**John Hidañl**

**Vice Chair**

**Jeff Haberman**

**Secretary/Treasurer**

**Kathy Prevost**

March 16, 2013

El Dorado County Planning Services  
Attn: Tom Dougherty, project Planner  
2850 Fairlane Court  
Placerville, CA 95667

**Subject: Application S12-0015/pd 12-0003 - Green Valley Convenience Center and Special Use permit request and Development plan for construction of the Center.**

The full APAC committee met on March 13th, 2013 and **voted unanimously (6-0) for conditional support for the project based on APAC conditions, which are included in the Development Conditions of Approvals.**

In general, APAC believes the project is an acceptable location for an ARCO station which includes an AM/PM market and a small drive thru food service (Schlotzky's Deli). This commercial project will help reduce the sales tax leakage into Sacramento County. The business plans for the site are appropriate for the area and could draw residents from outside of the County to the center. However, APAC has some major concerns regarding the project designs and entry points into the project. **The Conditional support is based on the following APAC conditions:**

- a. **No left turn be allowed for West bound traffic on Green Valley into the project. (See note 1)**
- b. **A right turn deceleration and acceleration lane must be added on East bound Green Valley road entrance to the project as well as other improvements. (See note 2)**
- c. **The reduction of the Wetland setback from 50 feet to 10 is not granted without additional mitigation. (See note 3)**
- d. **The traffic impact study is using questionable data and must be revised (See note 4)**
- e. **Onsite circulation should be reviewed for improved circulation and provisions (i.e. temporary use permit for the drive thru) defined to ensure that the drive through**

El Dorado Hills APAC - Non-partisan Volunteers Planning Our Future

Exhibit N

STAFF REPORT  
13-1347 K 31 of 39

**window does not impact traffic flow on either Sophia Parkway or Green Valley Road.  
(See note 5)**

- f. Project signage program should use back lighted signs and not exceed 12 feet (See note 6)**
- g. The 12 ft. high wall located on the edge of east property line should be designed to be aesthetically pleasing to the residents that will overlook the site.**

**Note 1** Driveway access on Green Valley Rd approx. 200 ft. north of curb return of Green Valley/ Sophia has significant Traffic Operational challenges. The traffic entering the proposed driveway from westbound GreenValley would be in the queue of the westbound Left turn from Green Valley to southbound Sophia Parkway. If a left into the proposed driveway was delayed as is a high probability due to the high volumes especially at peak hours this would dramatically affect moving traffic through the intersection and clearing the backup. This Traffic Operational issue is exacerbated with the geometric transition of the additional lane opening up on eastbound Green Valley Rd just west of the intersection. You now have vehicles transitioning into the added lane #2 and picking up speed with limited sight distance of just coming around the single lane and into two lanes. This would be an operational problem as well as a Traffic Safety issue to allow a left turn into this proposed driveway within 200 feet of the intersection. DOT should do a full Traffic Operational review of the proposed Commercial as this is just one of many operational issues that were identified in a site review. The Traffic study trip numbers do not include the increase in left turns on to Sophia Parkway when the new freeway interchange on to highway 50 at Empire Ranch is operational. The new interchange will have a major impact on the left turn lane at this intersection.

**Note 2** The site is deficient in clearing entry traffic from the roadway. There are no turn lanes that would remove slowing traffic from Green Valley Road as they enter the site. Traffic can be travelling east along GVR at 50 mph through the intersection and come upon cars at a near stop turning into the project.

At this same location, the addendum notes the criticality of the west bound left in to accommodate patrons' vehicles and 40' tandem delivery trucks (fuel). Many issues arise from this entry point that needs additional consideration.

- a.** If a 40 foot tanker is to cross two lanes of traffic into the project entrance, there should be a much greater site distance requirement due to the extended time of entry vs. a single automobile.
- b.** The turn pockets indicated to control queuing, utilize outdated TIA's based on the general plan and extrapolated data to 2025. Not only are these numbers outdated and an inaccurate representation of current traffic in the area, but the study neglects the effects of the opening of the HWY 50 interchange at Sophia parkway. When this occurs, the turn lanes suggested by this project will be will not have sufficient queuing capabilities and will not be able to make adjustments due to the raised median, "Left In" turn pocket created immediately aft of the Sofia Parkway left turn. A similar queuing issue will occur along Sophia blocking the right in right out entrance of the project.



### **Note 3 Wetland encroachment**

1. The wetland 'encroachment' is really more invasive than pictured – 6 feet to the 'water line' is a trampling, basically, during construction.
2. At completion there will be a 12' wall at 11' distance from the 'presumed' water line. Is the fence on top open, where people might toss fast food wrappers into the waterway? Or is it a solid fence all the way to 18', which neighbors *behind* this center will be looking at?
3. The mitigations do not mention the containment of oil/asphalt runoff – some containment should be mentioned, and the parking slope & 'edge' of the pavement becomes important (does water from parking run into the wetland?)
4. The Army Corps' definition of 'intermittent' is based on 30 days after the last major rain of the season. In 2007, that would have been about December. The satellite photo (<http://wdr.water.usgs.gov/wy2007/pdfs/390135121261001.2007.pdf>), shows green at the tail end of June. APAC feel skeptical that the Army Corps wouldn't care – maybe they don't – but the data is just sketchy.
5. The project site actually contains potential nesting sites for special-status birds, per the report. The mitigation (#2) says that IF a nest is found, it will be 'flagged' for a minimum 250'. What does 'flagged' mean, and is 250' a radius or diameter? Also, this buffer can be 'reduced' if there appears to be no disturbance to the nest – so if the mitigation is working, it is reduced until it is no longer working?

APAC really feel as though they are trying to cram too much onto this site. A gas station, sure, and associated food/deli; but drive through food and separate drive through carwash is too much. There just is not circulation space, particularly if they provide an actual reasonable setback to the wetland area.

APAC suggests elimination of the self-service carwash which will allow additional room on the side to meet the Wetland requirements and provide better traffic circulation on the side.

**Note 4** The methodology in determining Approved/Pending Projects (APP) is flawed in that the county staff provided only Wilson Estates, Green Valley Center and Dixon Ranch for review purposes. They neglected to evaluate the project with cumulative effects of Alto, Grand Amis, La Canada, Diamante estates, Green Springs Equestrian, Summer Brook, Silver Springs and the PA07-0067 commercial and 10 residence projects. All of these projects contribute to the congestion of Green Valley Road. (See measure Y below)

Page 7 paragraph 3 states annual volume adjustments were made based on 2% annual increase or an increase based on a straight line interpolation from 1998 to 2025. El Dorado Hills has already exceeded the GP growth projections of 2%. Population growth of EDC was 2% or 27,000 people. The El Dorado Hills growth exceeded 9% between 2000 and 2010 which was 24,000 people. 90% of the growth was in EDH. Traffic models based on the GP are deficient at best in this area. Recommendation would be to run a current traffic study for the project area.

Trip distribution needs to be based on a real time study performed at the project site. As a resident of this area, it is apparent that the project trip distributions in table 4 are an inaccurate representation of traffic flow in this area. West toward Folsom at 20% is low and East to EDH Blvd (South) is extremely high at 30%.

El Dorado Hills APAC - Non-partisan Volunteers Planning Our Future

Road improvements noted as mitigations are included in the CIP to be completed in the next 20 years (after 2021). This does not meet the community expectations for approval of this (or any other) project along this corridor.

**Note 5** On site Queuing: The drive through may block through traffic around the buildings and car wash when autos pass the ordering sign and proceed to the pickup window. APAC has regularly opposed development of fast food outlets outside the Highway 50 corridor. The proposed deli sandwich shop would be an exception. A Special use permit (or other provisions) must be established to preclude blocking traffic onto Sophia Parkway.

**Note 6** At best, signage is not attractive. What is the point of having more than minimum signage on a road which rarely sees a "new" driver, one who is not fully aware of all the businesses along his route? APAC recommend that the proposed sign be similar to the one at the ARCO station at the intersection of Foothill and Vineyard in Roseville, CA or not larger than the Chevron sign located up the street. The sign must be a back-lite non electronic display.

Overall APAC believe this is the right site for a commercial application of this type, but the site is too crowded with the addition of the car wash and has significant traffic safety issues (and environmental issues with the creek) that the developer needs to consider before moving forward seeking a project approval.

APAC may change its **position to non-support** if the concerns and conditions documented in this letter, are not implemented into the project design.

If you have any question about any of the comments and concerns expressed here, please contact John Hidahl, APAC Chairman at [Hidahl@aol.com](mailto:Hidahl@aol.com) or Norm Rowett GVC subcommittee Chairman at [arowett@pacbell.net](mailto:arowett@pacbell.net).

APAC appreciates having the opportunity to comment on this project.

Sincerely,



John Hidahl,  
APAC Chairman



Green Valley Road

Proposed Project Site

Green Valley Road



Eastern property line. RV storage to the left.

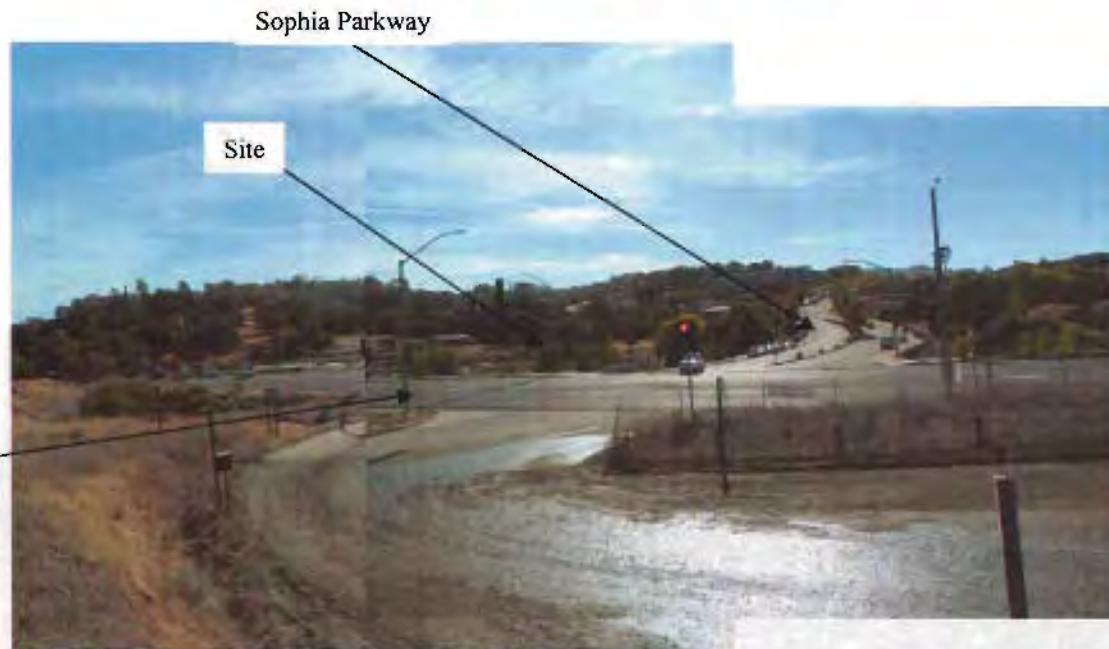
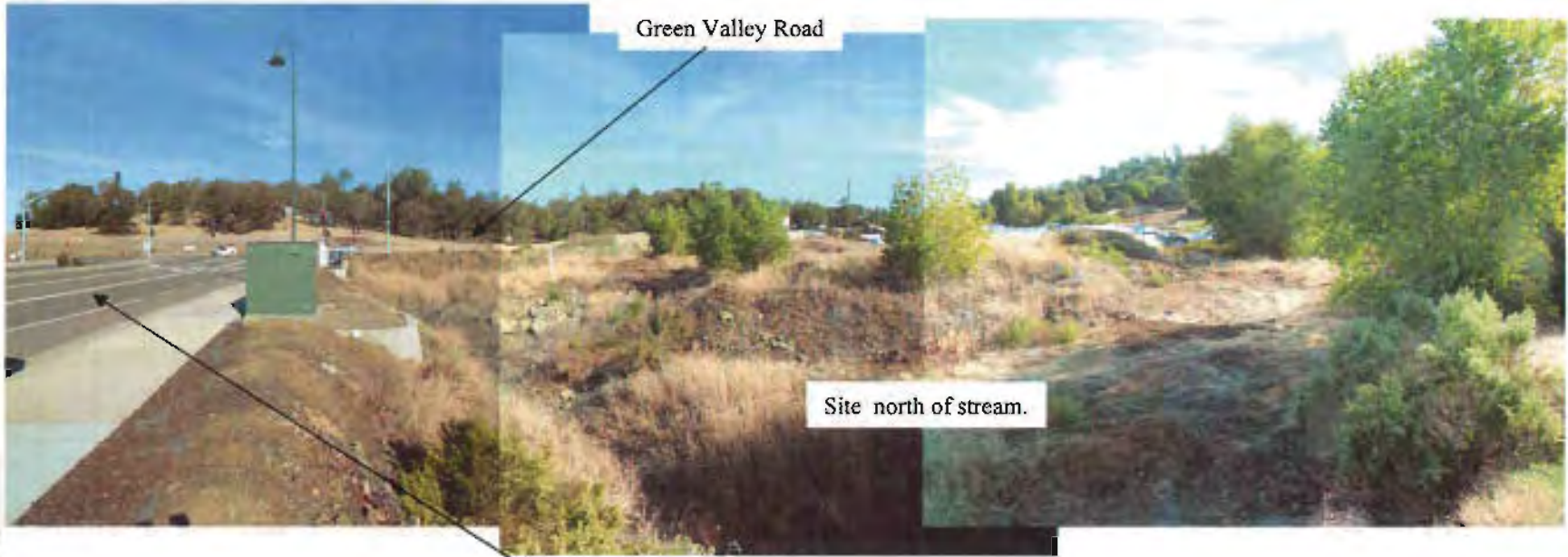


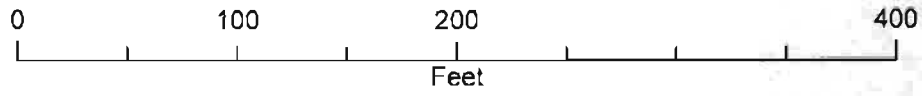
Exhibit O-3



Sophia Parkway

Site. Standing at the south corner.

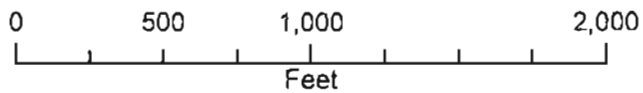




File Number PD12-0003

Exhibit P-1

STAFF REPORT  
13-1347 K 38 of 39



File Number PD12-0003

Exhibit P-2

STAFF REPORT  
13-1347 K 39 of 39