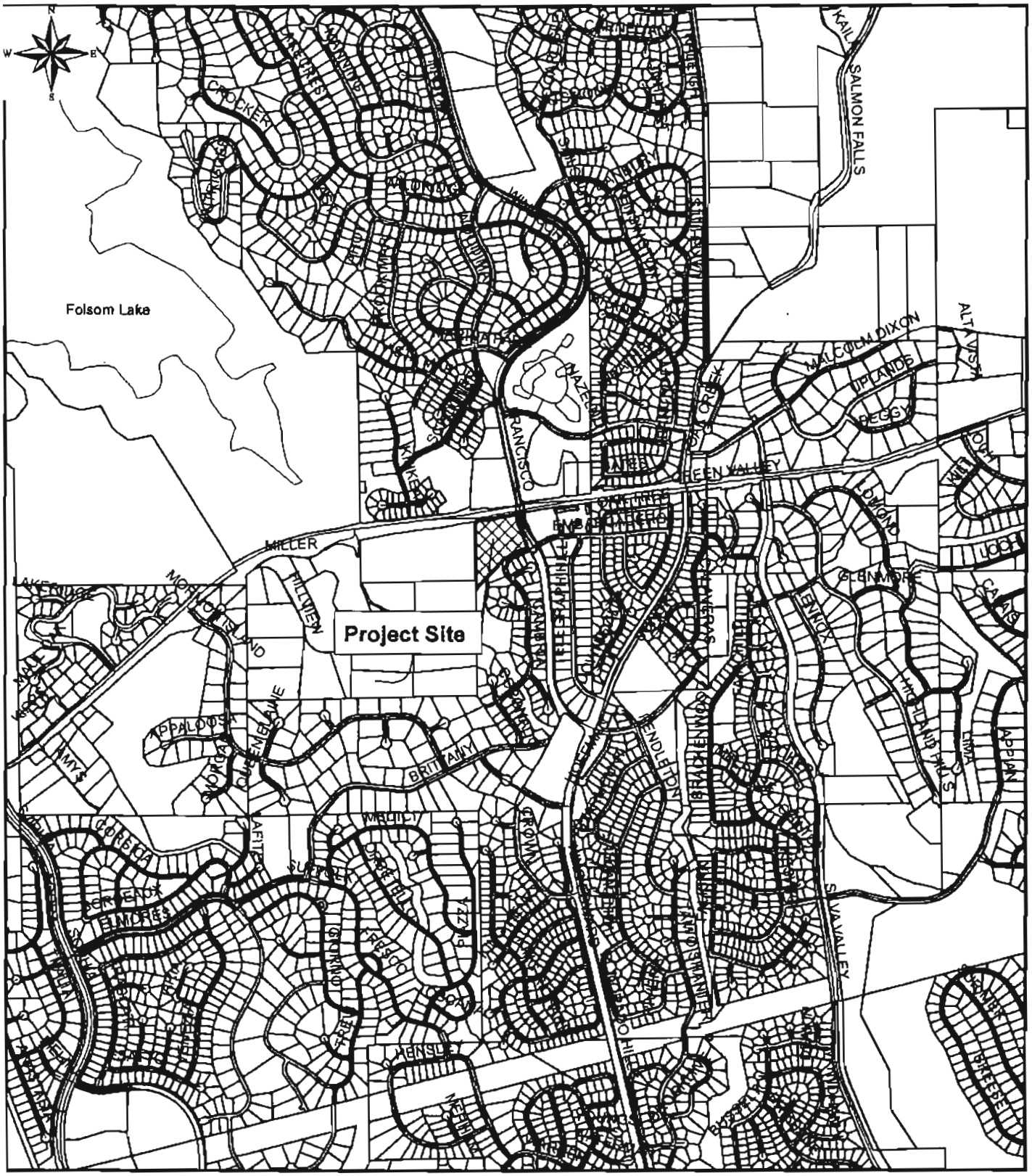
Green Valley/Francisco Commercial Center
File Nos. A11-0003, Z11-0004, PD11-0002, P11-0003



0 260 520 1,040 Feet

Map prepared by:
Mel Paballinas
El Dorado County
Development Services-Planning

Exhibit A- Location Map

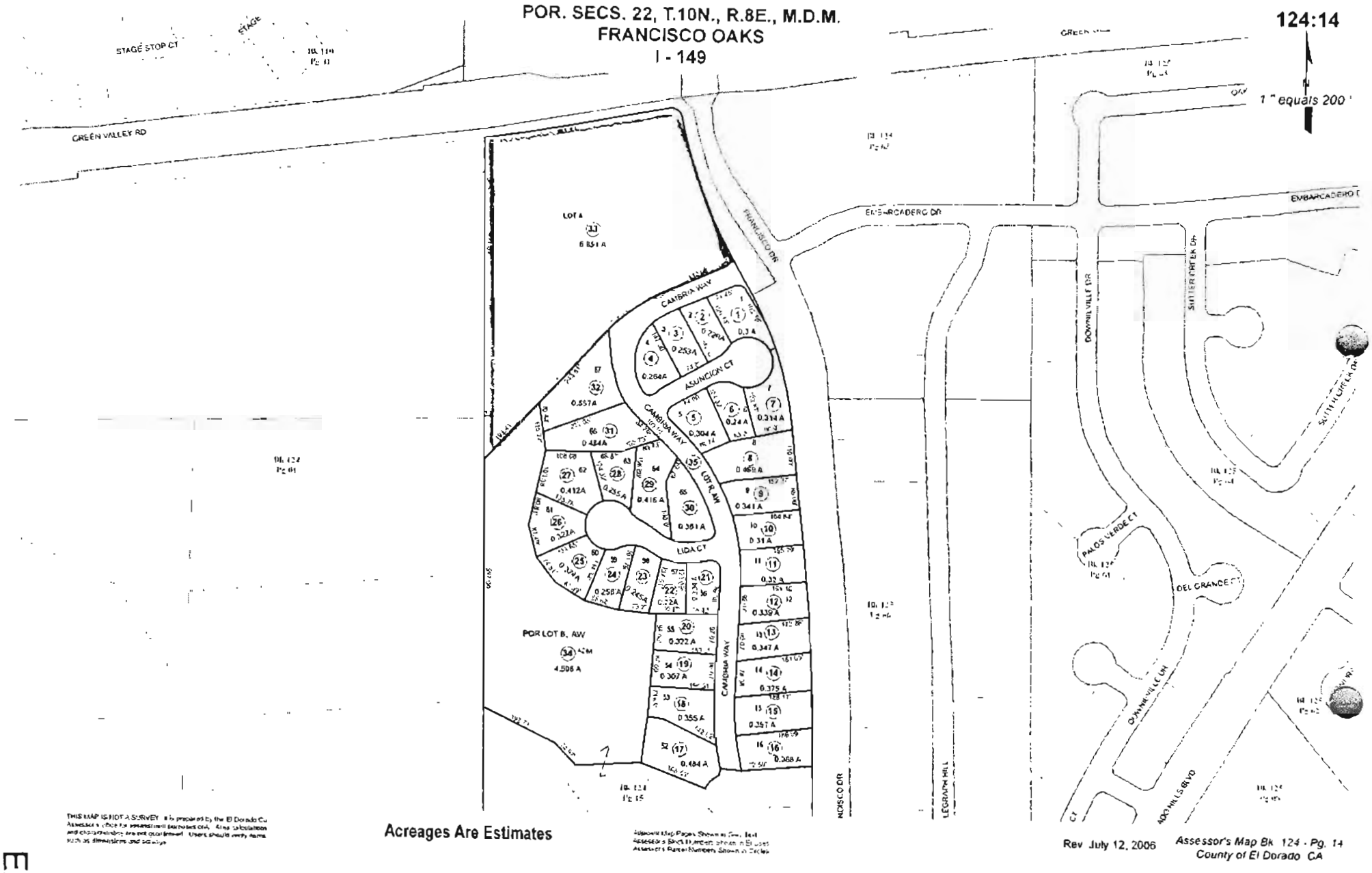
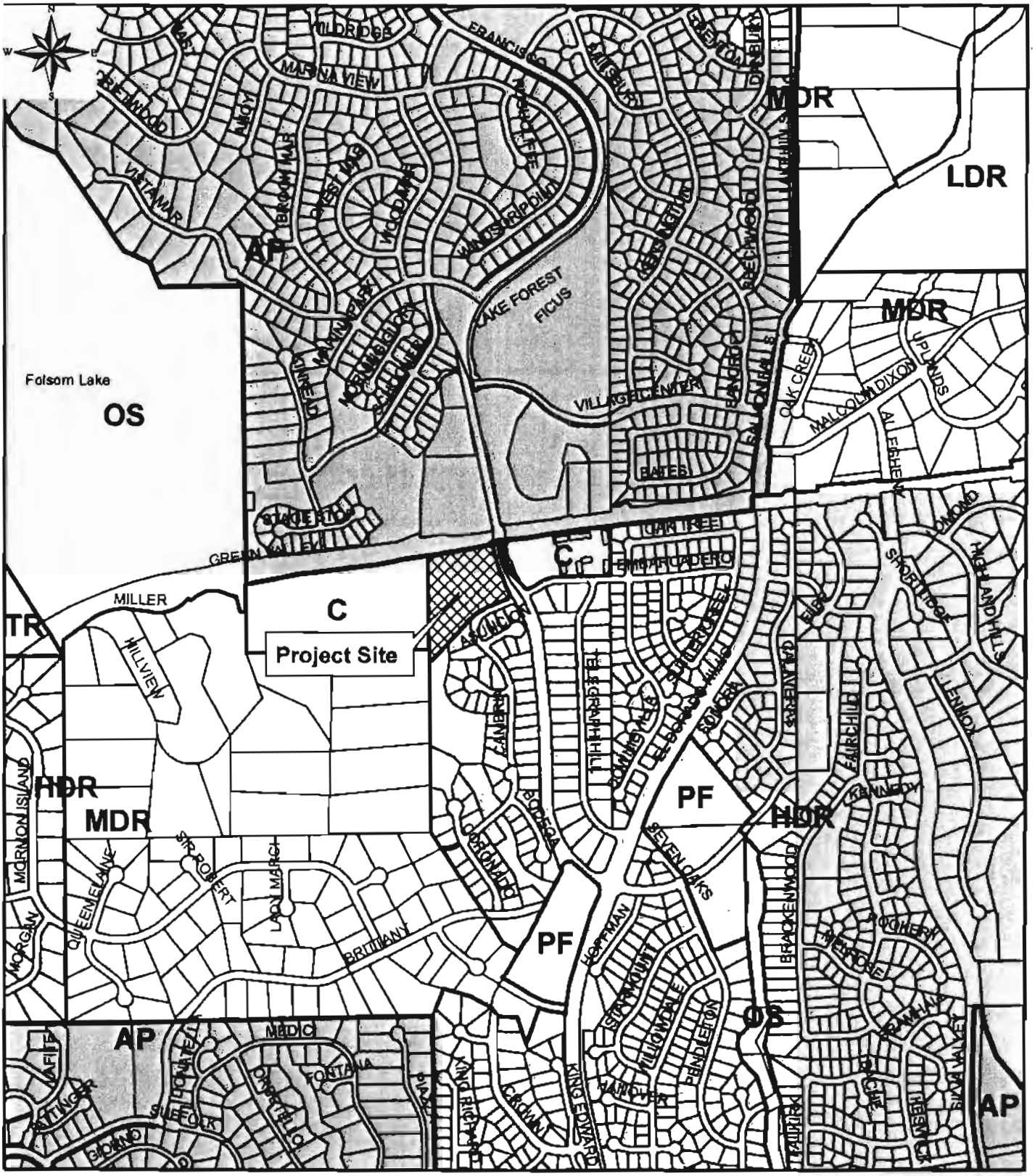

POR. SECS. 22, T.10N., R.8E., M.D.M.
FRANCISCO OAKS
I - 149
124:14
1 " equals 200 '
GREEN VALLEY RD
STAGE STOP CT
EMBARCADERO DR
FRANCISCO DR
CAMBRIA WAY
ASUNCION CT
LIDA CT
LOT A
POR LOT B, AW
DOWNIEVILLE DR
SUTTER CREEK DR
PALOS VERDE CT
DEL GRANDE CT
ADOBE HILLS BLVD
LEGRAND HILL
Acreages Are Estimates
Rev July 12, 2006
Assessor's Map Bk 124 - Pg. 14
County of El Dorado CA

EXHIBIT B

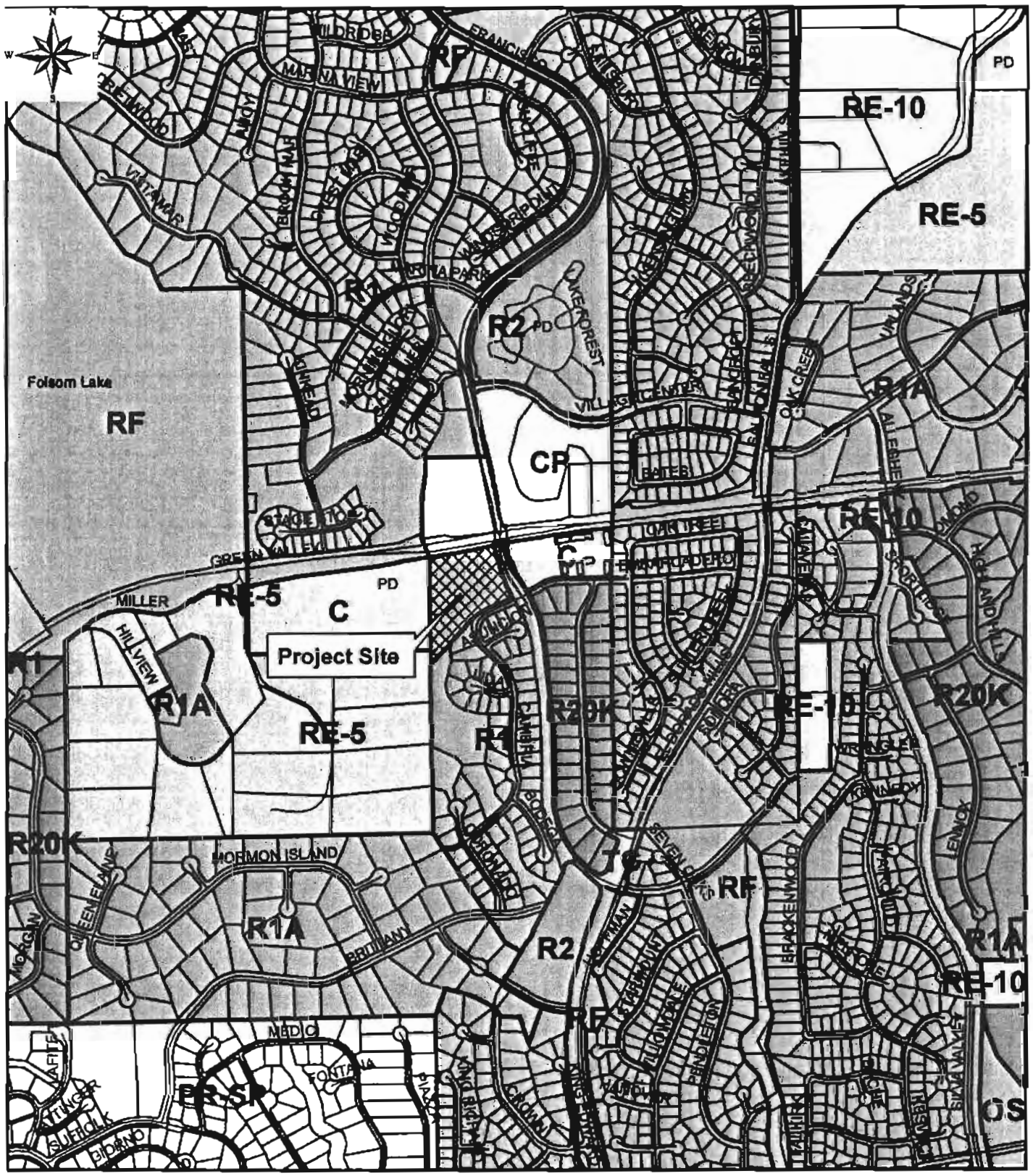
Green Valley/Francisco Commercial Center
File Nos. A11-0003, Z11-0004, PD11-0002, P11-0003



Map prepared by:
Mel Pabalinas
El Dorado County
Development Services-Planning

Exhibit C- General Plan Land Use Map

Green Valley/Francisco Commercial Center
File Nos. A11-0003, Z11-0004, PD11-0002, P11-0003



Map prepared by:
Mel Pabalinas
El Dorado County
Development Services-Planning

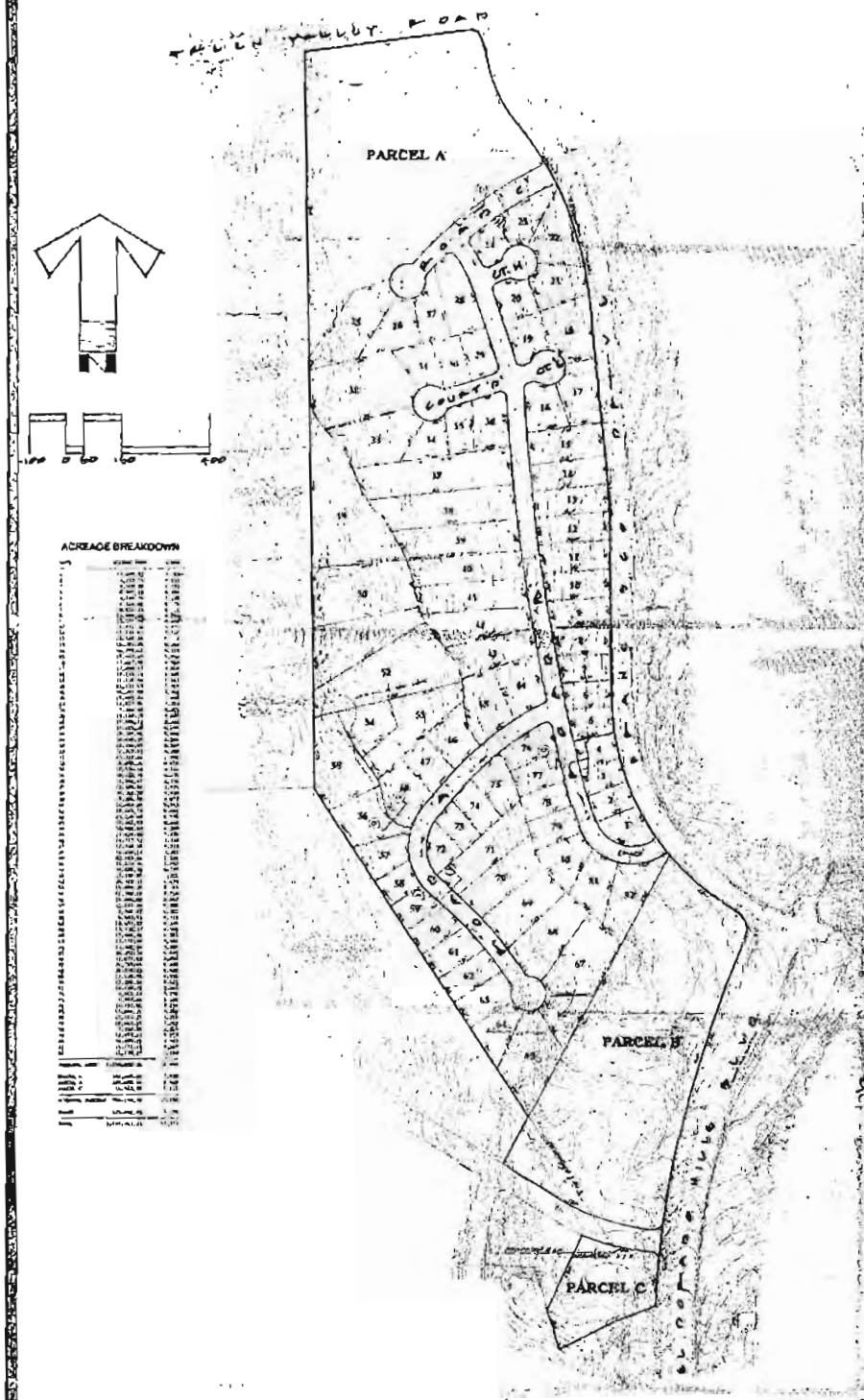
Exhibit D- Zone Map

REVISED TM88-1130

FRANCISCO OAKS TENTATIVE MAP

COUNTY OF EL DORADO
STATE OF CALIFORNIA

Gene E. Thomas and Assoc., Inc.



OWNER OF RECORD

SUBDIVIDER

ENGINEER

CONTOUR INTERVAL

SOURCE OF TOPOGRAPHY

SECTION, TOWNSHIP AND RANGE

ASSESSOR'S PARCEL NUMBER

PRESENT ZONING

PROPOSED ZONING

WATER SUPPLY AND SEWAGE DISPOSAL

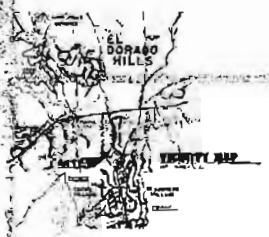
FIRE PROTECTION

SCHOOL DISTRICT

ENGINEER'S CERTIFICATE

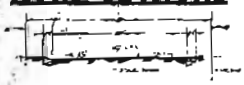
NOTES:

BASIS OF BEARINGS



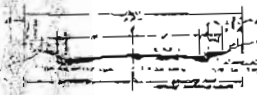
VICINITY MAP

ROAD SECTIONS

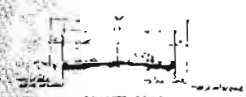


SECTION A-A

SECTION B-B



SECTION C-C



PRIVATE ROAD
SECTION D-D

EXHIBIT MAP

FRANCISCO OAKS & SURROUNDING LAND USE
PROPOSED PRIVACY GATES
EL DORADO HILLS

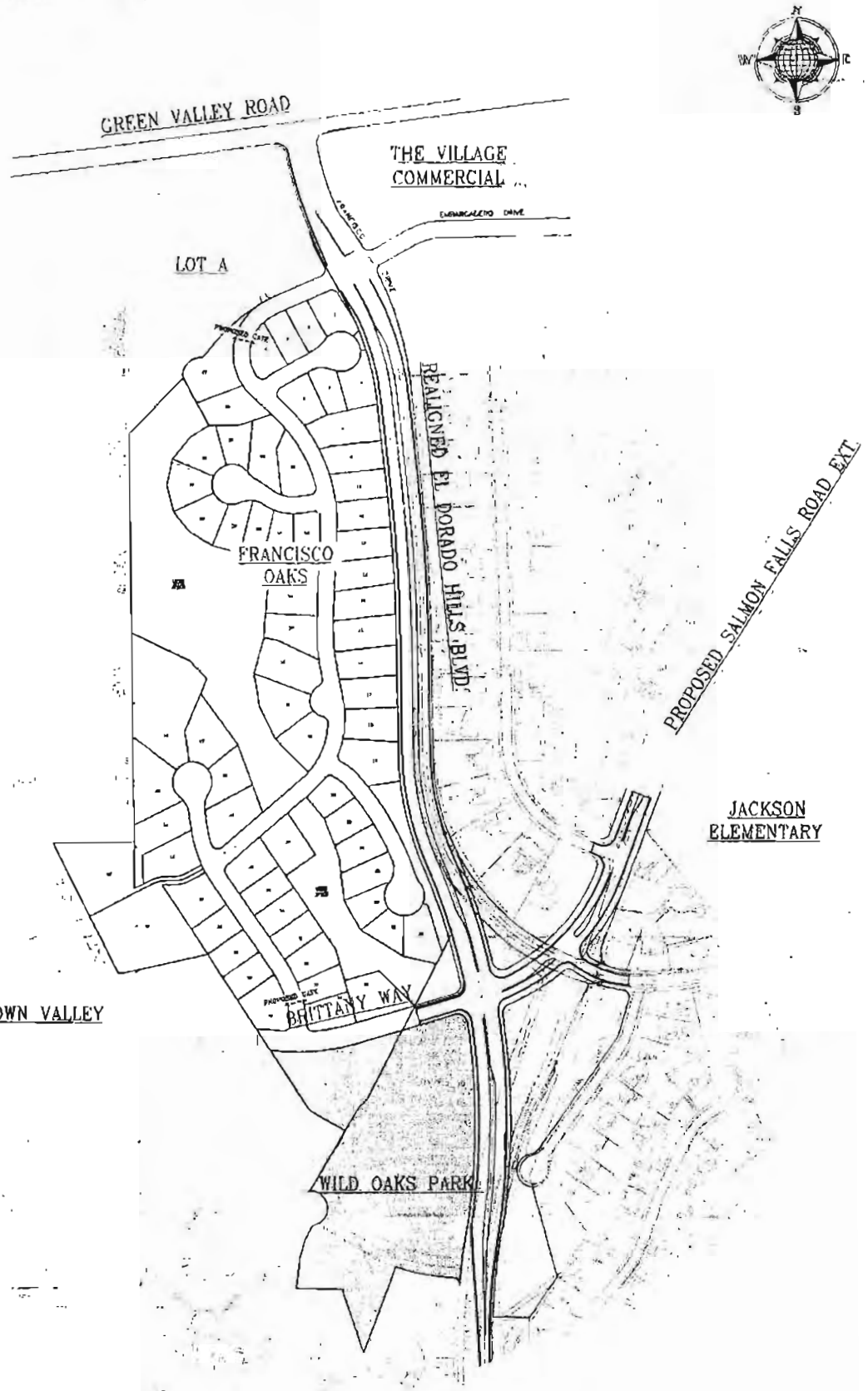


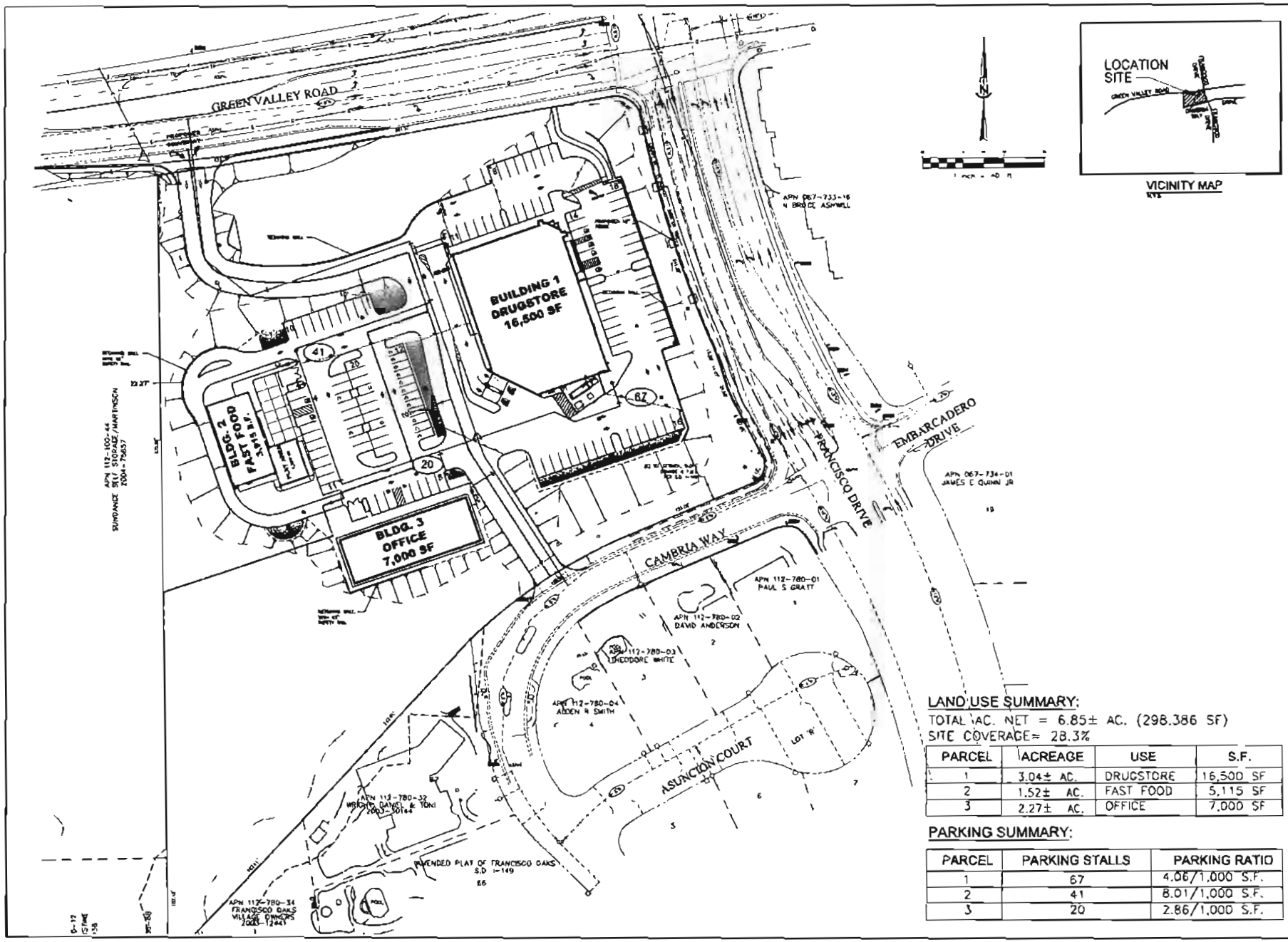
EXHIBIT F

Green Valley Commercial Center

Proposed Parcel		Proposed Commercial Buildings					
Parcel ID	Size (in acres)[B]	Bldg. ID	Floor Area (in square feet)	Proposed Use	Parking Stalls[A]	Maximum Height[C]	Notes
1	3.04	1	16,500	Retail Pharmacy with drive-thru	68	36'	Building 1 Pad at 612.65 feet elevation; Building coverage is 12%; Building 1 meets the minimum required setback (to the proposed parcel lines) in accordance with Section 17.32.040 of the Zoning Ordinance; Proposed monument signs along Green Valley Road and Francisco Drive (not to exceed 80 square feet)
2	1.53	2	5,115	Fast Food Restaurant (with play area) with drive-thru	41	30'- 6"	Building 2 Pad at 614 feet elevation; Building coverage is 3%; Building 2 meets the minimum required setback (to the proposed parcel lines) in accordance with Section 17.32.040 of the Zoning Ordinance
3	2.35	3	7,000	Office	20	22'	Building 3 Pad at 613 feet elevation; Building coverage is 7%; Building 3 meets the minimum required setback (to the proposed parcel lines) in accordance with Section 17.32.040 of the Zoning Ordinance

Notes: A. Total proposed parking is 129 stalls. Total required is 123 stalls per Section 17.18.060 of the Zoning Ordinance. Only one loading area is proposed (Building 1), which is a modification of the parking standards allowed as part of Planned Development; B. Minimum Commercial Lot Size= 5,000 square feet; C. Maximum Building Height= 50 feet as measured from finished building pads.

Exhibit G



LAND USE SUMMARY:

TOTAL AC. NET = 6.85± AC. (298.386 SF)

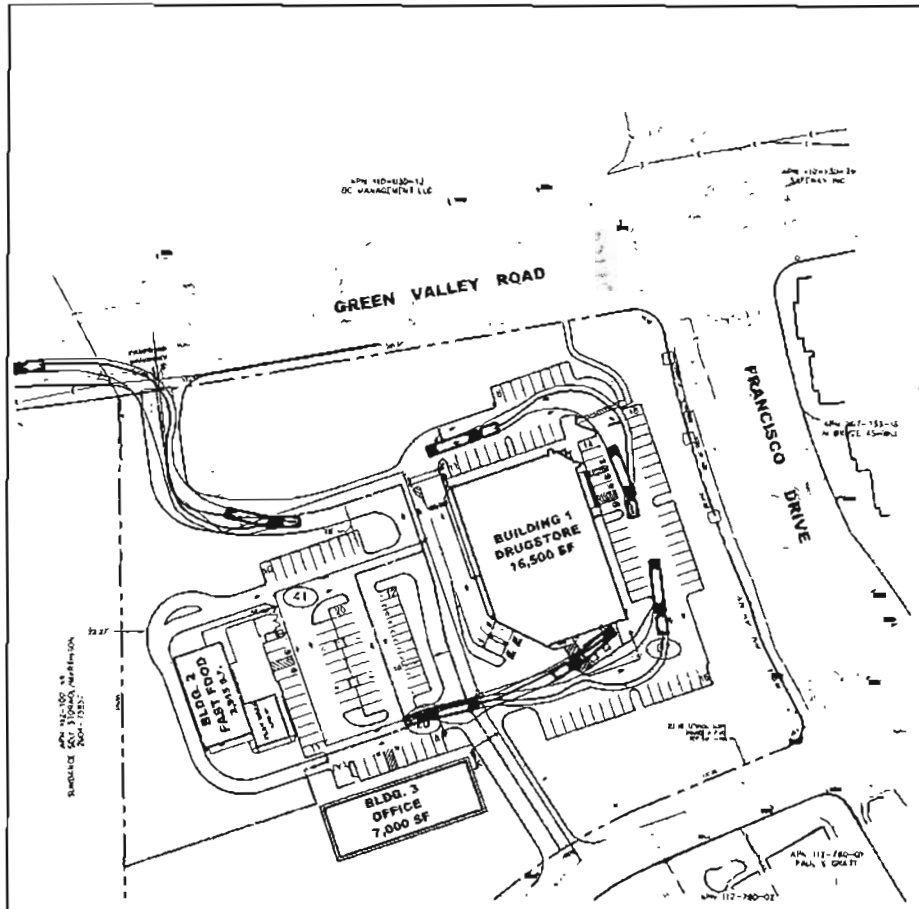
SITE COVERAGE= 28.3%

PARCEL	ACREAGE	USE	S.F.
1	3.04± AC.	DRUGSTORE	16,500 SF
2	1.52± AC.	FAST FOOD	5,115 SF
3	2.27± AC.	OFFICE	7,000 SF

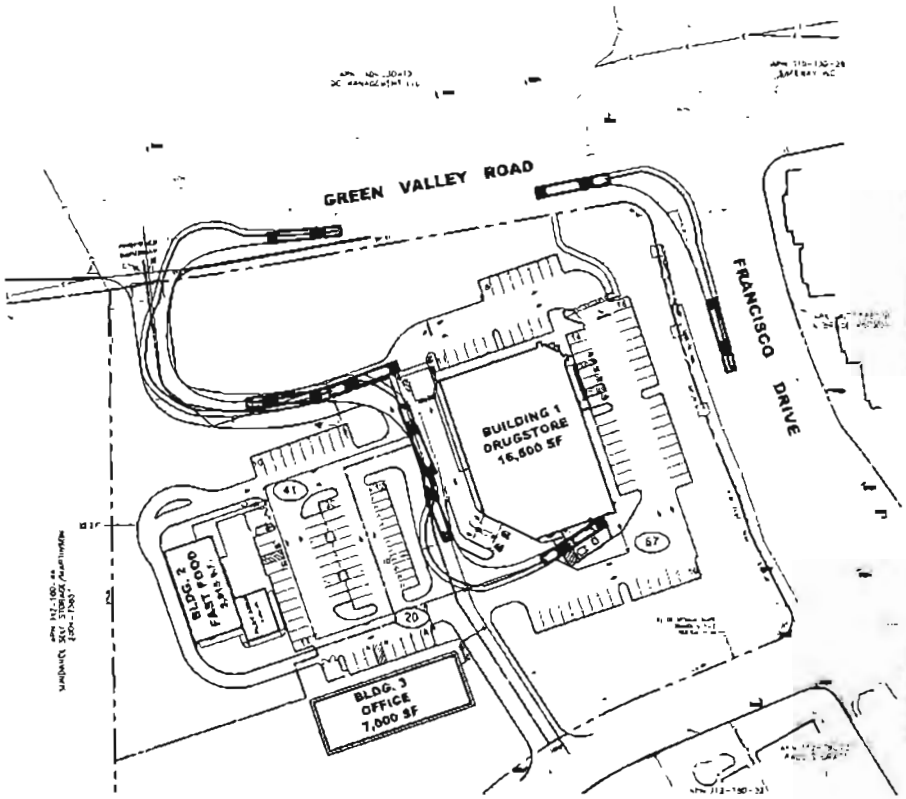
PARKING SUMMARY:

PARCEL	PARKING STALLS	PARKING RATIO
1	67	4.06/1,000 S.F.
2	41	8.01/1,000 S.F.
3	20	2.86/1,000 S.F.

EXHIBIT H



TRUCK TURN IN MOVEMENTS



TRUCK TURN OUT MOVEMENTS

WINN COMMUNITIES

RSC ENGINEERING INC

TRUCK TURN EXHIBIT
GREEN VALLEY CENTER

GREEN VALLEY RD & FIRST DR.
COUNTY OF EL DORADO

SHEET TITLE
SP

SHEET NO
1
OF 8

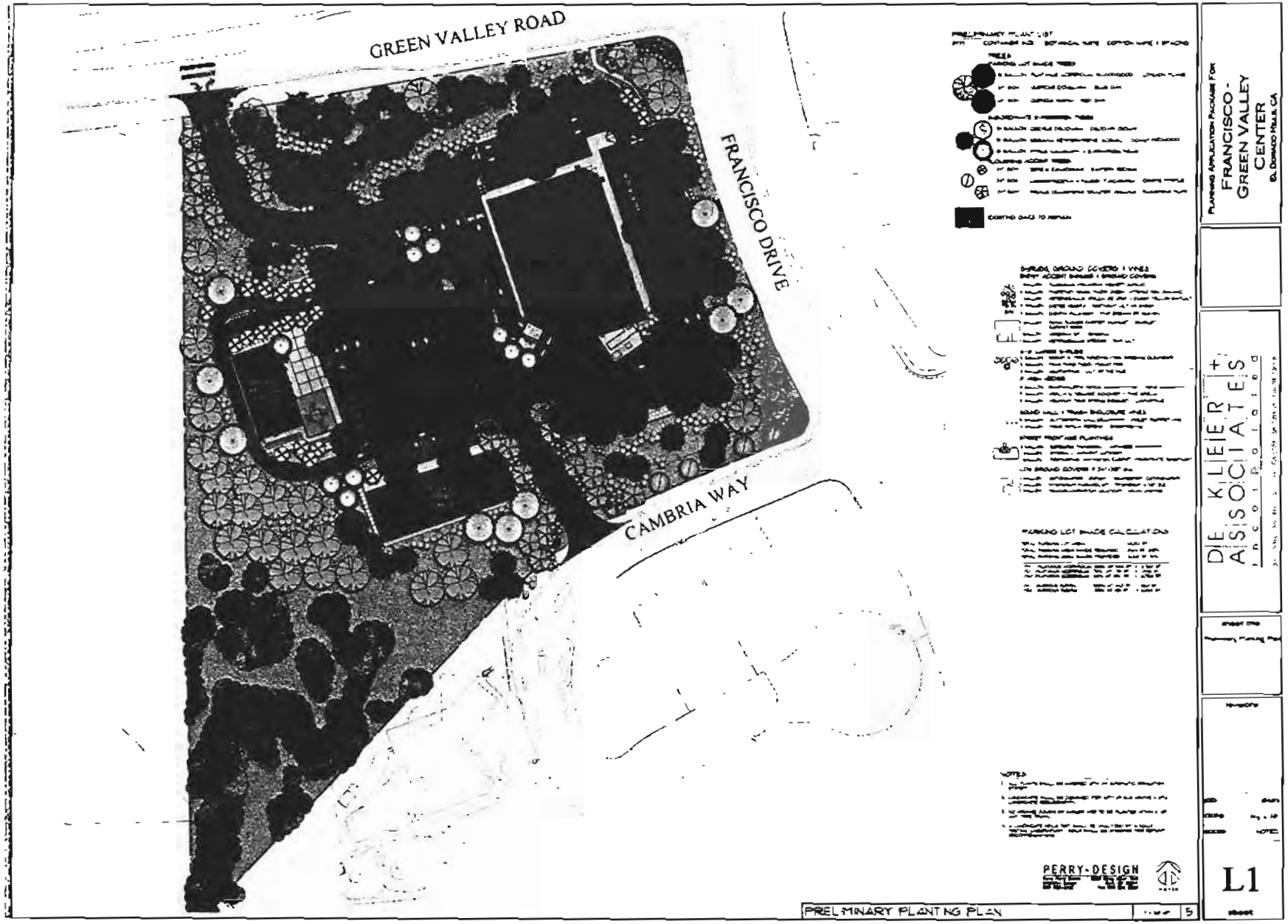
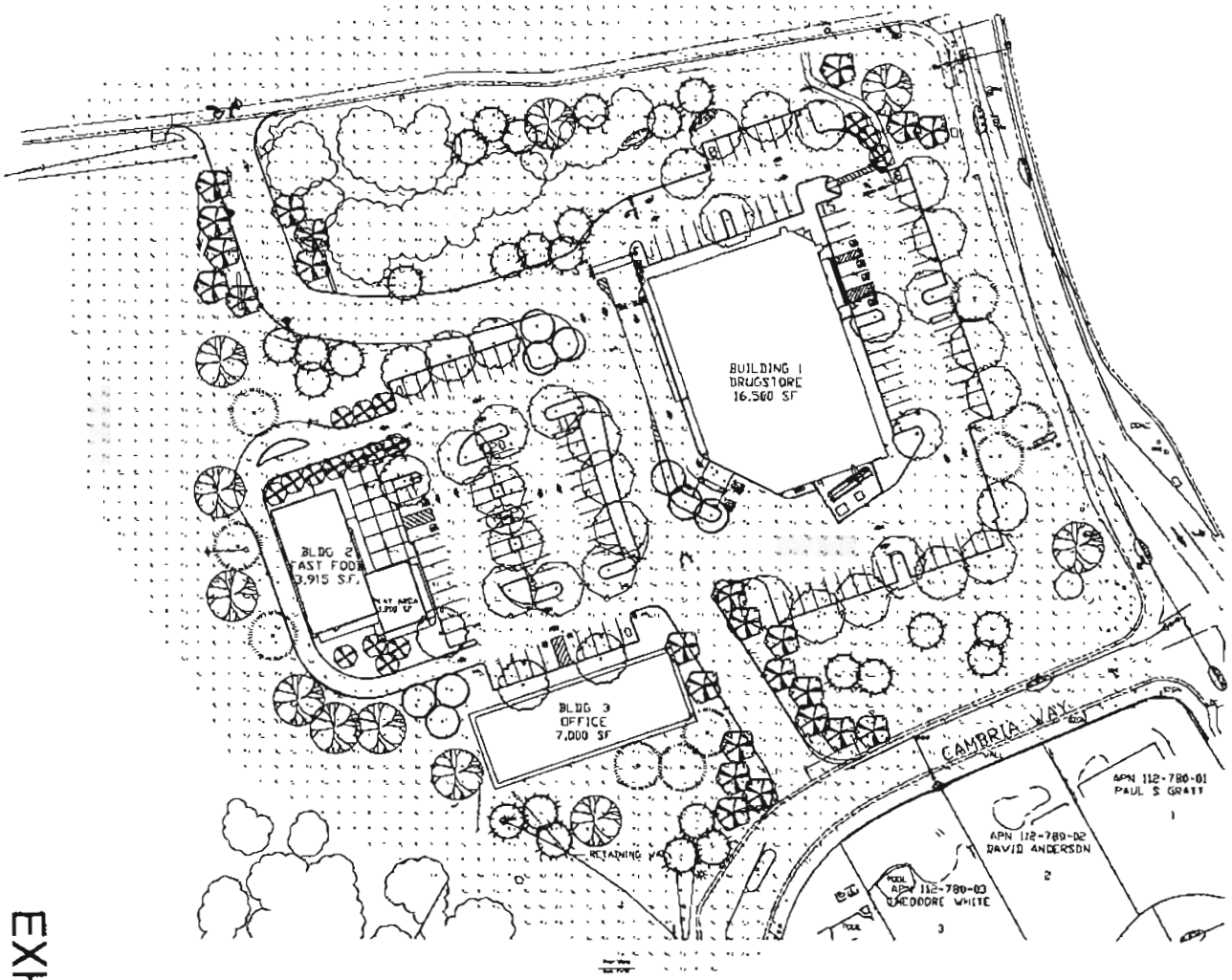

GREEN VALLEY ROAD
FRANCISCO DRIVE
CAMBRIA WAY
PRELIMINARY PLANTING PLAN
PERRY-DESIGN
DE KLEIER + ASSOCIATES
Incorporated
FRANCISCO-GREEN VALLEY CENTER
L1

EXHIBIT I



LUMINAIRE SCHEDULE

Symbol	Label	Qty	Catalog Number	Description	Lamp	Mounting	Voltage	LLF	Notes

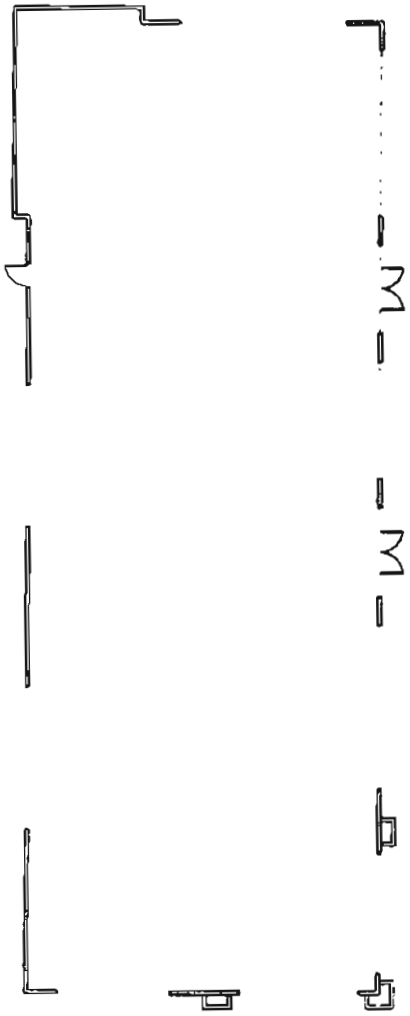
STATISTICS

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min

LUMINAIRE LOCATIONS

No.	Label	X	Y	Z	MH	Orientation	Tilt	Lamps	Lumens	LLF	Notes

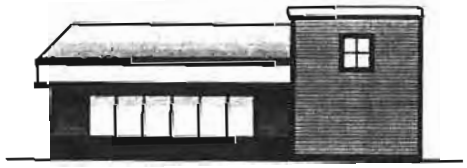
P 11-0003/PD 11-0002



Floor Plan

North Elevation

South Elevation



West Elevation

East (Francisco Drive) Elevation

EXHIBIT K

FRANCISCO - GREEN VALLEY

Winn Communities

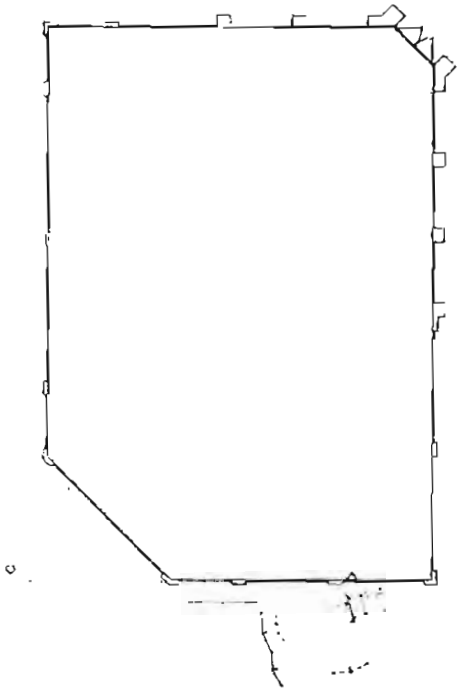
DE KLEER + ASSOCIATES

South Elevation

North Elevation

East Elevation

West Elevation



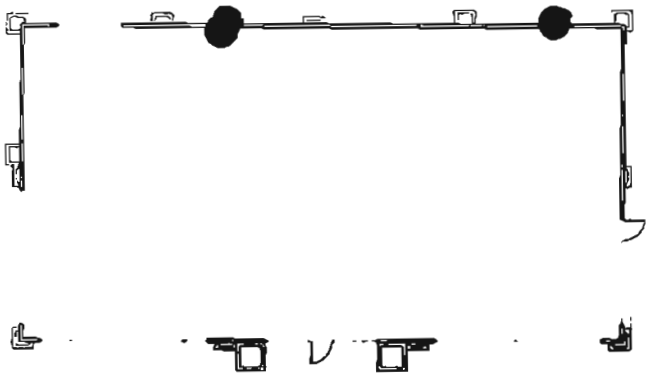
Floor Plan

FRANCISCO - GREEN VALLEY

Winn Communities

DE KLEER + ASSOCIATES

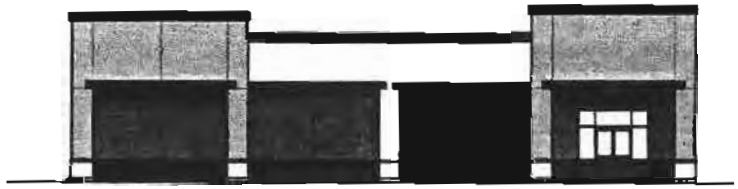
P 0003/PD 11-0002



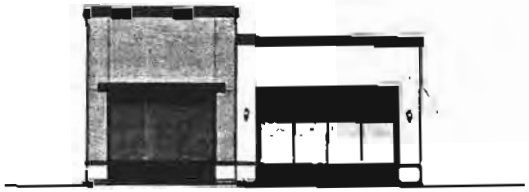
Floor Plan

East Elevation

North Elevation



West Elevation



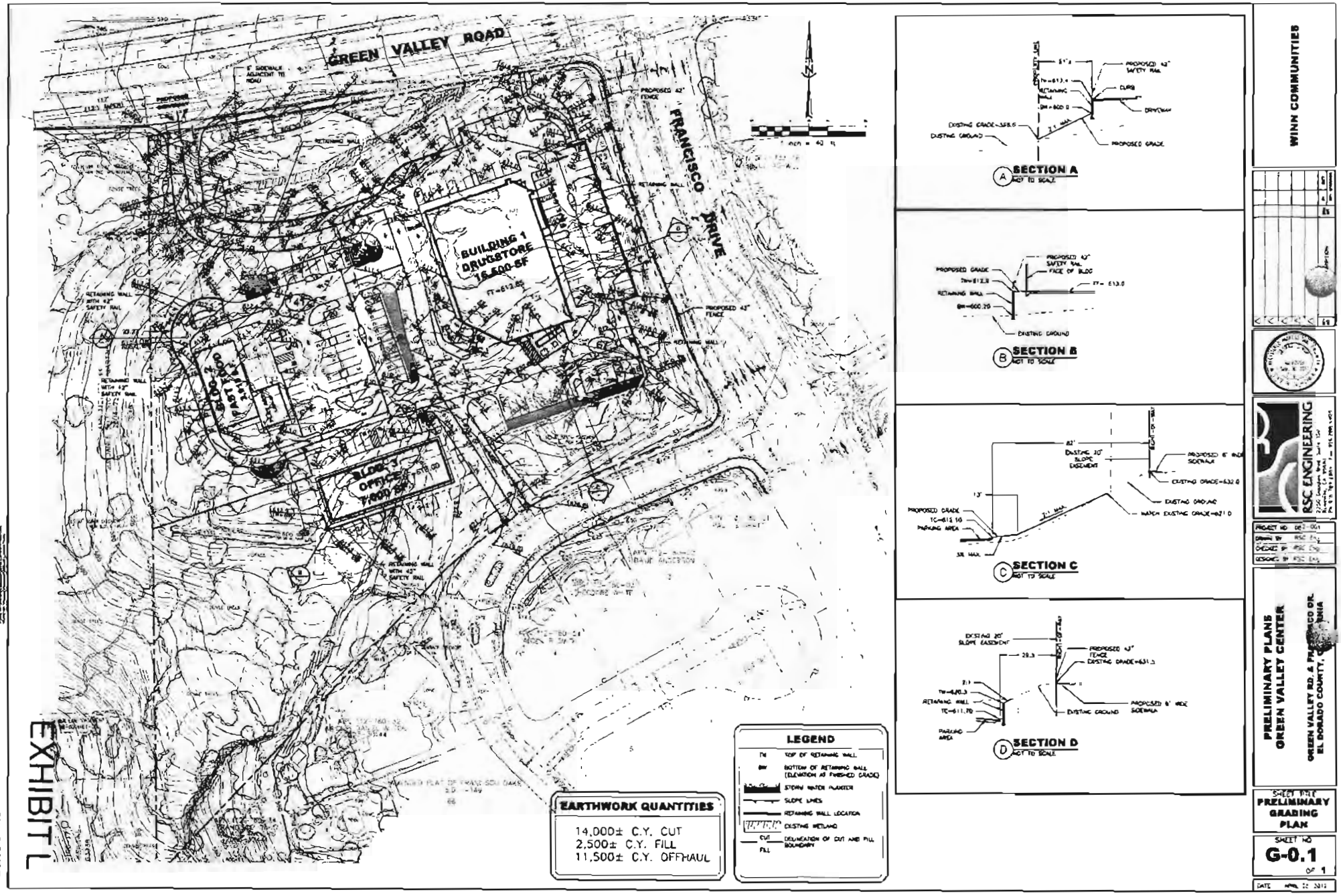
South Elevation

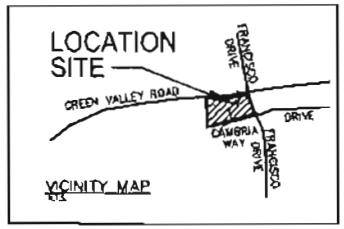
FRANCISCO - GREEN VALLEY

Winn Communities

DE KLEER + ASSOCIATES

01-0003/PD 11-0002


GREEN VALLEY ROAD
FRANCISCO DRIVE
BUILDING 1
DRUGSTORE
16,600 SF
BUILDING 2
RETAIL
8,000 SF
SLOPE J
OFFICE
7,600 SF
SECTION A
SECTION B
SECTION C
SECTION D
EARTHWORK QUANTITIES
14,000± C.Y. CUT
2,500± C.Y. FILL
11,500± C.Y. OFFHAUL
LEGEND
WINN COMMUNITIES
RSC ENGINEERING
PRELIMINARY PLANS
GREEN VALLEY CENTER
PRELIMINARY GRADING PLAN
G-0.1
EXHIBIT L



LEGEND

229-3"
EXISTING TREE CANOPY WITH TRUNK SIZE & TYPE

EXISTING TREE CANOPY TO BE REMOVED

REPLACEMENT AREA (TOTAL 0.61 AC)

A = 579 SF
TOTAL 579 SF

TYPICAL CALLOUT EXAMPLE
A "TREE #"-[QUAT] #229-3"

NOTE:
1. SIZE OF PROJECT.
2. TOTAL SITE CANOPY = 183,772 SF = 3.4 AC.
3. ALLOWABLE REMOVAL = 30% = 0.7 AC
4. REMOVAL PER PLAN = 2.7 AC
5. REDUCTION PER ARBORIST RECOMMENDED REMOVAL DUE TO HEALTH AND NON NATIVE TREES ARE = 0.8 AC.
6. NON NATIVE & POOR HEALTH OAK TREES RECOMMENDED FOR REMOVAL BY ARBORIST EXCLUDED FROM CANOPY LOSS CALCULATIONS AND NOT TO BE REPLACED.
29, 45, 48, 69, 73, 80, 98, 101, 112, 199, 221, 322, 214, 233, 235, 239, 249, 270, 281, 283, 285, 293, 301, 308, 309, 310.
NON-REPLACEABLE CANOPY AREA IS 0.8 AC
7. TOTAL HEALTHY OAK CANOPY LOSS = 2.7-0.8 SF = 2.1 AC.*
8. MITIGATED AREA REQUIRED = 1.1 AC
200 TREES (ONE GALLON) AT 200 TREES/AC DENSITY OR 840 ACORNS AT 800 ACORNS/AC DENSITY.
9. REMAINING CANOPY IS A FUNCTION OF THE FOLLOWING:
A. SITE PLAN
B. TREE HEALTH
10. TREE TAGS AND INFORMATION FROM INITIAL ARBORIST REPORT AND INVENTORY SUMMARY GATED WAY 2006, PREPARED BY SIERRA NEVADA ARBORISTS FOR SPECIFIC TREE AND CONDITIONAL ASSESSMENTS.

1 inch = 50 ft

WINN COMMUNITIES

RSC ENGINEERING

PROJECT NO. 047-001
DRAWN BY: RSC Eng
CHECKED BY: RSC Eng
DESIGNED BY: RSC Eng

TREE PRESERVATION PLAN
FRANCISCO/GREEN VALLEY
COMMERCIAL CENTER
GREEN VALLEY RD & FRANCISCO DR.
COUNTY OF EL DORADO

SHEET TITLE
EXH

SHEET NO
1
OF 1

DATE: MARCH 29, 2011

A 11-0003/Z 11-0004

FILE COPY

EXHIBIT M

Francisco – Green Valley Center

Green Valley Road at Francisco Drive
El Dorado Hills, California

SIGNAGE GUIDELINES

July 6, 2011

Planning Files P11-0003 & PD11-002

Landlord:

Family Real Property
1130 Iron Point Rd., Ste. 150
Folsom, CA 95630

Prepared By:

DE KLEER + ASSOCIATES
incorporated

Exhibit N

1. Table of Contents

A. General Criteria

1. Design Intent
2. Approval and Permit
3. General Requirement
4. Specific Design Criteria
5. Administration
6. Prohibited Signs
7. Temporary Signs

B. Signage

1. Signage Site Plan
2. Monument Signs
3. Ornamental Banners
4. Wall Signage – OFFICE
5. Wall Signage – FAST FOOD
6. Wall Signage – RETAIL
7. Wall Signage Criteria
8. Miscellaneous Signs

A. General Criteria

1. Design Intent:

The guidelines of this program are designed to complement architectural elements of the existing buildings and coordinate the type, placement, and physical dimensions of signs within the shopping center thereby appearing as an integral part of the center.

In cases not covered by this Signage Program, the prevailing criteria will follow the County of El Dorado Sign Ordinance in force.

2. Approvals and Permit:

A. Each User or Lessee will be provided **by the owner's representative** with a copy of these Signage Guidelines and criteria as their first step in obtaining signs within the Center. Compliance with this signage program will be strictly enforced. Any non-conforming or unapproved sign installed by the User must be brought into compliance at the User's expense.

B. These criteria shall not imply that any governmental approval will be automatically granted. The User is solely responsible, at its own expense, for obtaining any and all required approvals from governmental agencies and shall obtain all permits from the County of El Dorado Planning Department and Building Department.

General Requirements:

C. Each User is required to submit to The Approving Authority for approval **before fabrication**, at least four (4) copies of detailed design drawings indicating the location, size, copy layout, colors, materials, finishes, illumination, and method of attachment.

D. All permits for signs and their installation shall be obtained by User or User's representative, at User's sole expense prior to installation.

E. All signs shall be constructed, installed and maintained at User's sole expense.

F. All signs shall be designed consistent with the County of El Dorado's adopted sign design guidelines for signs, except to the extent modified by this Sign Program.

G. Users shall be responsible for fulfillment of all governmental requirements and specifications, including those of the **County of El Dorado** and California Electric Code.

F. All signs shall be reviewed for compliance with the above mentioned criteria, as well as processed through a secondary review concerning overall design quality. Approval or disapproval of sign submittals based on aesthetics of design shall remain the right of the Approving Authority, its representative and the County of El Dorado.

4. Specific Design Criteria:

A. All signs should meet or exceed all current applicable building codes (i.e. electrical, structural, etc). Signage should meet all requirements of the State of California and the County of El Dorado, except to the extent modified by this Sign Program.

B. Sign content shall be limited to business identification signs only; products or service shall not be displayed on any permanent signage.

C. Wall signs shall be individual pan channel letters constructed of aluminum backs and returns with acrylic faces and internal neon / LED lighting.

D. All exterior signs shall be secured by stainless steel, nickel, or cadmium plated fasteners.

E. All exposed fasteners to be painted to match the background surface.

F. NO exposed wireways, transformers, electrical boxes, switches, wiring, conduit, lamps, tubing or access doors of any kind are allowed.

G. All exterior signs exposed to the weather shall be peg-mounted a minimum of ½" away from the wall finish to allow for drainage, unless otherwise specified.

H. All signs attached to building wall or fascia shall be connected to a junction box provided by The Approving Authority, with the final electrical hook up and connections by User's sign contractor. All signs shall be controlled by The Approving Authority's time clock.

I. All penetrations of the building structure by User's sign contractor required for sign installation shall be neatly sealed and watertight.

J. All identification labels shall be concealed, except where required by code. An Underwriter's Label is required on all electrical signage.

K. Sign contractor shall repair any damage caused by their work. Damage to structure that is not repaired by the sign contractor shall become the User's responsibility to correct.

L. User shall be fully responsible for the operation of their sign contractor, and shall indemnify, defend and hold the Approving Authority, its representative, and all parties harmless from damages or liabilities on account thereof.

M. Sign surfaces that are intended to be flat shall be without oil canning, or other visual deformities.

N. The general location of wall signs shall be centered vertically and horizontally on fascias, unless otherwise specified. Signs shall not cover or interrupt major architectural features.

5. Administration:

A. The **"Approving Authority"** shall be Family Real Property, LP or its designee and the **"User"** shall be any tenant or business occupying one of the buildings on site.

B. The amount of hours per day during which the signs will be illuminated shall be determined and controlled at the Approving Authority's sole discretion.

C. The Approving Authority reserves the right to hire an independent electrical engineer (at User's sole expense) to inspect the installation of all signs, and reserves the right to require that any discrepancies and/or code violations be corrected at User's expense.

D. The sign contractor shall carry workman's compensation and public liability insurance against all damage suffered or performed against any and all persons or property while engaged in the construction or erection of signs in the amount of $1,000,000 per occurrence.

E. At the expiration, or early termination of a tenant's lease term, that tenant shall be required to remove their signs, cap off the electrical connection, patch the fascia and paint the entire fascia area to match the surrounding areas at the tenant's expense within seven (7) days.

F. Sign contractors shall be advised (by User) that no substitutes will be accepted whatsoever unless so indicated in specification and approved by the Approving Authority and User. Any deviation from these specifications may result in the rejection of the sign by User and/or the Approving Authority.

G. In the event any conflict in the interpretation of these guidelines cannot be satisfactorily resolved, the Approving Authority's decision shall be final and binding upon the User.

6. Prohibited Signs:

A. No sign shall be installed, relocated or maintained so as to prevent entry or exit out of any door. No sign shall create a safety hazard by obstructing view of pedestrian and vehicular traffic.

B. No sign shall be located within a required easement, unless an encroachment permit has been authorized by the affected utilities.

C. No sign shall obstruct access to fire hydrants, fire department connections, or fire department access roads.

D. Signs on/or affixed to trucks, automobiles, trailers or other vehicles which advertise, identify, or provide direction to a use or activity not related to its lawful making of deliveries or sales of merchandise or rendering of services from such vehicles are prohibited when such vehicles are located on the Shopping Center.

E. Signs, which audibly advertise, identify or provide direction to a use or activity, are prohibited.

F. It is unlawful for any User to exhibit, post or display or cause to be exhibited, posted or displayed upon any sign, anything of an obscene, indecent, or of immoral nature or unlawful activity.

H. Painted wall signs are prohibited.

I. Cabinet wall signs are prohibited, except for recognized corporate logos, which should be embossed type cabinets wherever allowed by the morphology of the logo.

J. Permanent advertising devices such as attraction boards, posters, banners and flags, except where approved by the Approving Authority, its representative and the County of El Dorado.

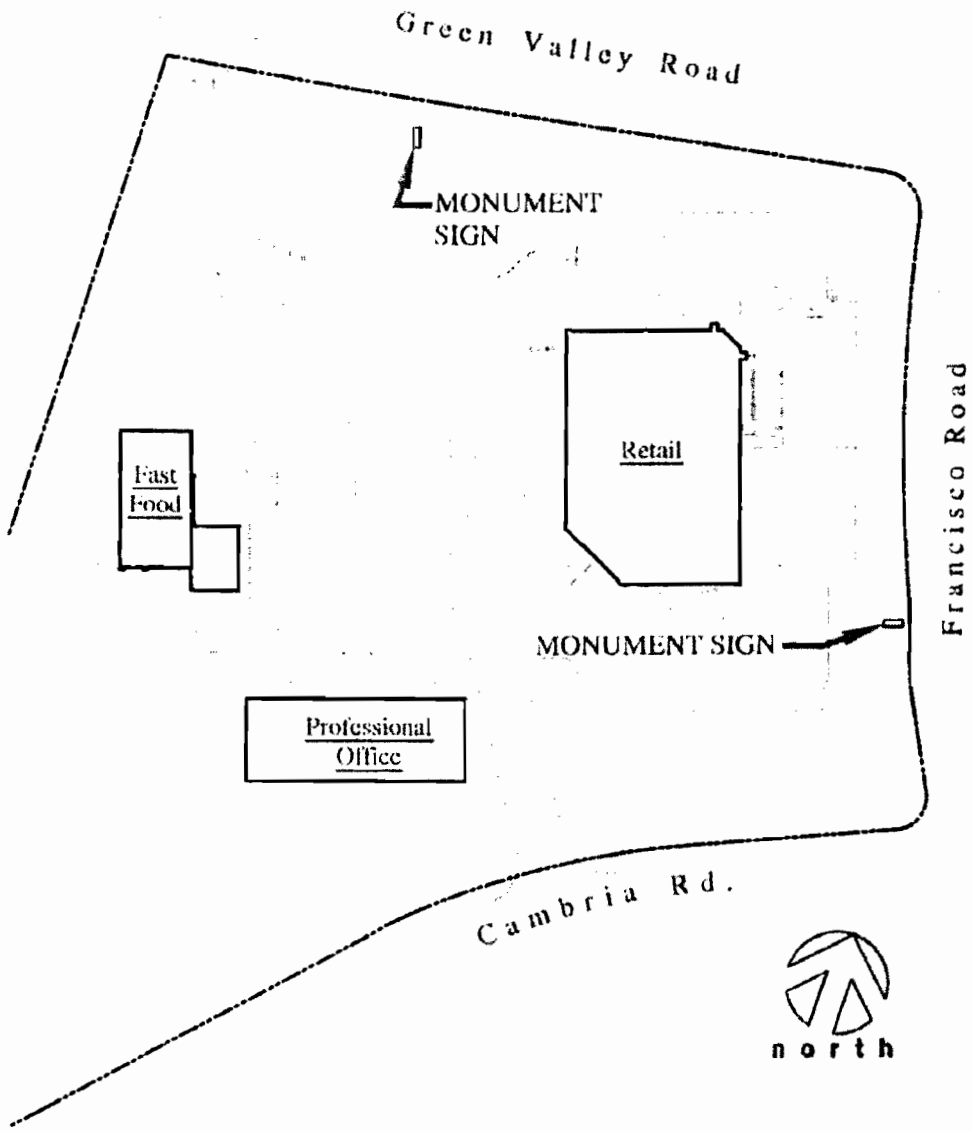
K. Window signs except where approved by the Approving Authority, its representative and the County of El Dorado.

7. Temporary Signs:

A. Temporary wall signs, leasing signs, window signs, pennants, banners or flags, inflatable displays or sandwich boards will be allowed, subject to owner's approval, if consistent with provisions in the County of El Dorado Sign Ordinance.

B. Signage

1. Signage Site Plan



2. Monument Signs

CENTER IDENTIFICATION SIGN:

Configuration: Freestanding structure to match architecture of the center.

Location: Facing Green Valley Road and facing Francisco Drive.

Quantity: Two

Letter Size: 12" high max letters

Overall size: 8'-0" H x 6'-6" W

Illumination: External / Light Fixture on Column

USER PANELS:

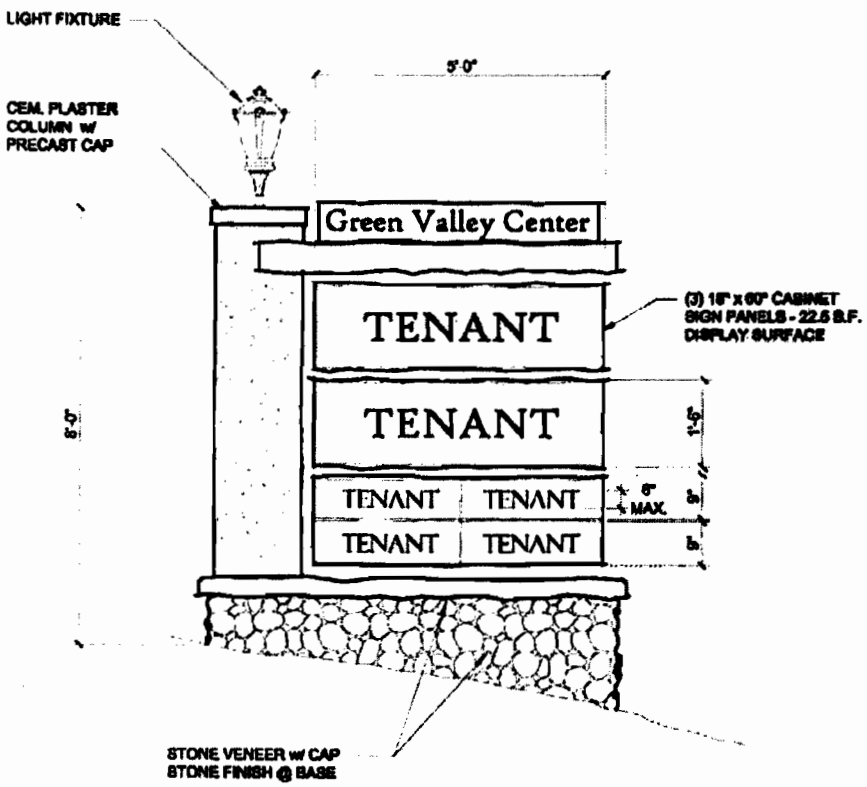
Location: Inside Monument sign as shown.

Quantity: 3, with option to divide panels into one four smaller panesl as shown.

Letter Size: 12" high letters maximum

Panel size: 60" W maximum x 12" D maximum x 1'-6" H typical, two-sided.

Illumination: Internal illumination.



3. Ornamental Banners

Configuration: Full color banners attached to light poles.

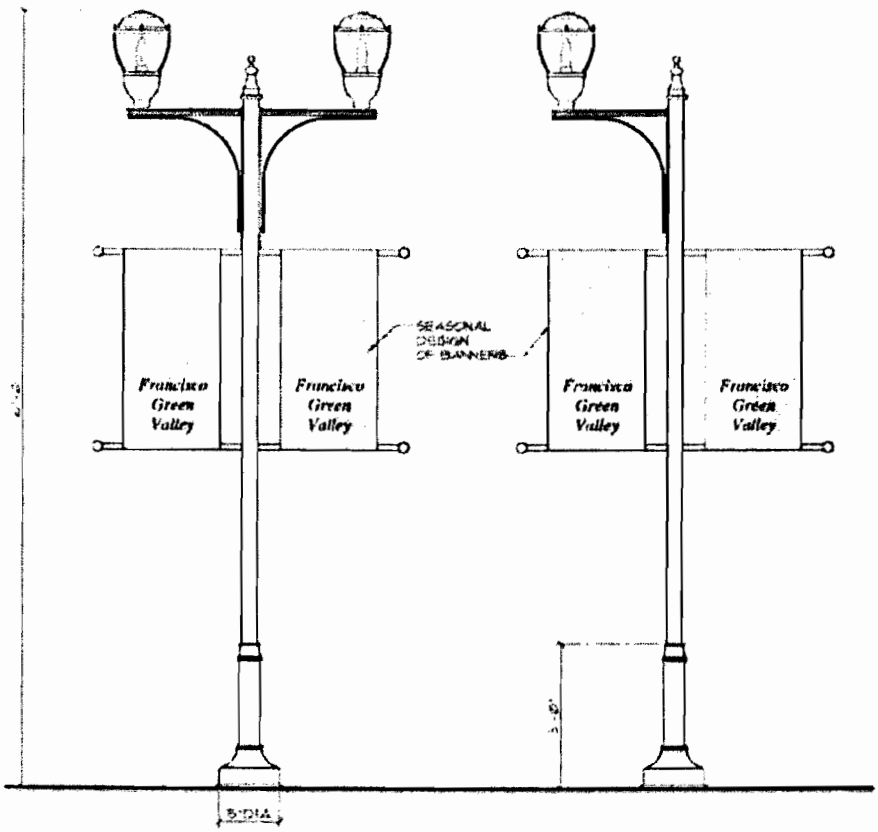
Size: Overall size 7'-0" H x 2'-O" W

Illumination: Light Poles

Content: Graphic elements

Quantity: TBD

Location: Parking Lot



4. Wall Signage

OFFICE BUILDING

Configuration:
Illuminated dimensional letters and/or logo mounted to fascia.

Content:
User Identification

Location:
Office building facades.

Signage Area:
Up to a maximum of 3 square feet signage per lineal frontage of the façade upon which the sign is located.

Primary User ID:
Sign: 30" max. high letters/logo

Length of Sign:
Not to exceed 75% of lineal frontage

Quantity:
2 each per User (maximum one per elevation).

5. Wall Signage

FAST FOOD PAD

Configuration:
Illuminated dimensional letters and/or logo mounted to fascia.

Content:
User Identification

Location:
Fast Food building facades.

Signage Area:
Up to a maximum of 3 square feet signage per lineal frontage of the façade upon which the sign is located.

Primary User ID:
Sign: 30" max. high letters/logo

Length of Sign:
Not to exceed 75% of lineal frontage

Quantity:
2 each per user (maximum one per elevation).

6. Wall Signage

Configuration:
Illuminated dimensional letters and/or logo mounted to fascia.

Content:
User Identification

Location:
Retail / Drugstore Building

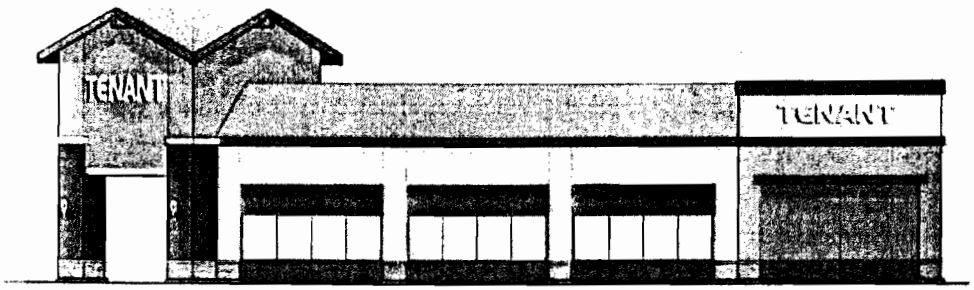
Signage Area:
Aggregate area not to exceed 250 sq. ft. for the primary elevation. Aggregate area not to exceed 100 sq. ft for the secondary elevation.

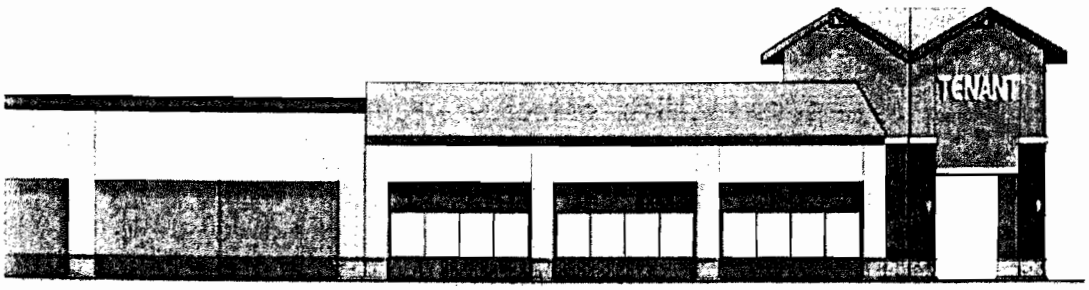
RETAIL BUILDING

Primary User ID:
4' maximum high letters/logo mounted directly to the wall for the primary elevation. 3' maximum high letters/logo for the secondary elevation.

Length of Sign:
Not to exceed 70% of lineal frontage

Quantity:
2 (maximum one per elevation). The angled Main Entry Tower fascia shall constitute its own separate elevation.





7. Wall Signage - Criteria

A. Design

The intent of the sign guidelines is to provide a fast and equitable framework through which the users can employ creativity in their signs, yet preserve the harmony and community themes of the center.

B. Wall Mounted Signs

1. Individual internally illuminated pan-channel letters with aluminum returns, translucent acrylic faces, trim-caps and internal neon illumination
2. Individual open face channel letters with aluminum returns and exposed neon lighting
3. Individual reverse channel letters with aluminum faces and returns and halo lighting
4. Plain cabinet (box type) signs are not allowed

C. Calculation of Sign Area:

1. Area of individual letters shall be calculated according to a simple rectilinear figure that contains a word or group of words.

D. Copy and Color:

1. All User signage shall consist solely of the User's trade name, logo and recognizable trademark insignia.
2. Sign copy shall not include products or services except, as it may legally be part of the User's trade name, logo and recognizable trademark insignia.
3. Sign colors and the entire display shall be approved by (Owner).
4. Colors may conform to the Users color scheme. No fluorescent or "day glow" colors permitted.
5. The owners reserve the right to disallow colors to be used that are inconsistent with the building colors and overall theme colors of the shopping center.

8 . Miscellaneous Signage

A. **On Site Directional Signage** - The Fast Food and Retail Users shall be allowed a maximum of four (4) directional signs, subject to restrictions of the County of El Dorado Sign code, each not exceeding four (4) square feet in area and a height of four (4) feet. Said directional signage shall only contain that information necessary for onsite circulation, parking and site information without any advertising. All costs associated with the installation of these signs, including any electrical requirements, shall be at the sole cost of the User(s) of the respective buildings.

B. **Typical Storefront Vinyl Lettering (Optional)** – Each user shall be permitted to place upon or adjacent to their entrance, no more than 144 square inches of vinyl Scotchcal #3680-20 White Lettering, letter style Helvetica. Application shall not exceed 2" in height, indicating hours of business, emergency contact information, etc.

OWNER:
FAMILY REAL PROPERTY
1130 IRON POINT ROAD, SUITE 150
FOLSOM, CA 95630
ATTN: GEORGE M. CARPENTER, JR.
(916) 355-1450

APPLICANT:
WINN COMMUNITIES
1130 IRON POINT ROAD, SUITE 150
FOLSOM, CA 95630
ATTN: GEORGE M. CARPENTER, JR
(916) 355-1450

ENGINEER:
RSC ENGINEERING, INC
2250 DOUGLAS BLVD., SUITE 150
ROSEVILLE, CA 95661
ATTN: RICHARD S. CHAVEZ, P.E.
(916) 788-2884

SCALE:
1" = 50'

CONTOUR INTERVAL:
1 FOOT

SOURCE OF TOPOGRAPHY:
RADMAN AERIAL SURVEY 05-30-05

SECTION, TOWNSHIP & RANGE:
SECTION 22, TOWNSHIP 10N, RANGE 8 EAST

ASSESSOR'S PARCEL NO.
124-140-33

EXISTING ZONE
R1-PD

PROPOSED ZONE:
COMMERCIAL-PLANNED DEVELOPMENT (C-PD)

PROPOSED LAND USE DESIGNATION
COMMERCIAL-PLANNED DEVELOPMENT (C-PD)

TOTAL ACREAGE
6.85 ACRES

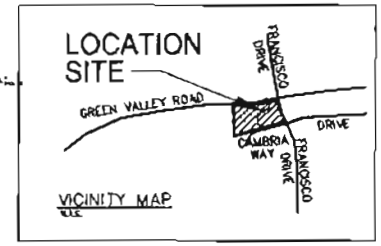
TOTAL EXISTING PARCELS
1

TOTAL PROPOSED PARCELS:
3

MAXIMUM PARCEL = 3.04 AC
MINIMUM PARCEL = 1.53 AC

UTILITIES:
SEWER - EL DORADO IRRIGATION DISTRICT
WATER - EL DORADO IRRIGATION DISTRICT
GAS & ELECTRICITY - P.G.&E.
TELEPHONE - AT&T
CABLE - COMCAST
WASTE DISPOSAL - EL DORADO DISPOSAL
FIRE - EL DORADO HILLS FIRE DEPARTMENT

Notes
1. Refer to final parcel map for accurate lot dimensions and configuration.
2. The property lies within Zone "X" on FIRM Map No. 06017C Panel 0704E dated Sept. 26, 2008. Zone "X" is an area outside the 500 year flood plain.
3. See grading & drainage plan and utility plan for drainage and hydrant improvements.
4. See tree preservation plan for location, type and size of all existing trees on site including quantity of trees to be removed.



LEGEND:

- EXISTING BOUNDARY
- PROPOSED PARCEL LINES
- EXISTING EASEMENTS
- EXISTING WETLANDS
- ABANDON PORTION OF NO EGRESS INGRESS RIGHTS LINE
- ABANDON PORTION OF 20' SEWER EASEMENT
- ABANDON PORTION OF 15' DRAINGE EASEMENT
- APPROX. LOCATION OF PROPOSED 15' DRAINAGE EASEMENT

Zoning Administrator: _____
Approval/Denial Date: _____
Board of Supervisors: _____
Approval/Denial Date: _____

WINN COMMUNITIES

RSC ENGINEERING

TENTATIVE PARCEL MAP
FRANCISCO/GREEN VALLEY
COMMERCIAL CENTER
GREEN VALLEY RD & FRANCISCO DR.
COUNTY OF EL DORADO

SHEET TITLE
PM

SHEET NO.
1
OF 9

DATE: APRIL 25, 2012

Exhibit O

El Dorado Hills
Area Planning Advisory Committee
1021 Harvard Way
El Dorado Hills, CA 95762

A11-0003 & Z11-0004/Winn

2011 Board
Chair
John Hidahl
Vice Chair
Jeff Haberman
Secretary/Treasurer
Alice Klinger

July 13, 2011

El Dorado County Planning Services
Attn: Pierre Rivas for Gina Paolini, Project Planner
2850 Fairlane Court
Placerville, CA 95667

Subject: ~~Z~~ A 11-0003/ 11 0004 - WINN GP 7 REZONE (Family Real Property LP/Winn Communities/RSC Engineering, Inc.)

Dear Pierre,

The El Dorado Hills Area Planning Advisory Committee (APAC) met on Wednesday, July 13, 2011 and reviewed the Winn Communities application for a General Plan Amendment and Rezone. **Members then voted unanimously (7 to 0) on a motion of non-support for this application.** Included in this motion were the following concerns:

A. Resident Opposition. During both of the APAC meetings at which the application was discussed, a significant number of residents expressed their views and most in attendance were strongly opposed. Residents of Francisco Oaks Village have circulated a petition to express opposition. Of the more than 130 people contacted, only one did not sign.

B. Property Values. Without an appropriate buffer between the commercial activity and residences, the adverse impact upon the property value of the latter is certain to be substantial.

C. Traffic. Vehicular traffic, already at level F at the intersection of Green Valley Road and Francisco Drive at peak hours, will be yet more congested and traffic patterns more hazardous. Of particular concern is the increased hazard for children walking and cycling to Marina Village School.

D. Air Quality. The impact of fast food cooking odors and exhaust from cars idling at two drive-through sales points will have a significant negative impact upon air quality in the neighborhood.

E. Tree Preservation and Grading. In contrast to the extensive grading and consequent tree removal that would result from a rezone and commercial development, residential development would entail substantially less grading, save more significant trees and preserve more of the existing suburban atmosphere.

F. Deed Restrictions. CC&R's specify that the property be developed for residential use.

Exhibit P

G. Fast Food Location. APAC has regularly opposed development of fast food outlets outside the Highway 50 corridor.

If the APAC position on this application does not prevail and the application is granted, APAC recommends strongly that the following conditions be placed upon eventual development:

A. Architectural Style. The style should be consistent for all three buildings, and no significant alteration should be permitted to meet the demands of a fast food style franchise.

B. Visual Pollution. Free standing signs should be low-profile, non-lighted monument style. Signs on building faces should be back lighted, low intensity and without animation. No signs at all should be permitted on south facing building elevations. Architectural controls should prohibit installation of banner signs outside or inside facing outward.

Area lighting fixtures must face downward and be of a design that prevents seepage from the property.

Mechanical equipment on roofs to be screened.

C. Sound Pollution. Drive through speakers must be shielded and directed so as to prevent seepage to the adjacent residential areas.

D. Water Pollution. Water sediment collection ponds shall be maintained and landscaped to fit natural landscape or proposed constructed landscape.

E. Traffic Mitigation. Applicant must me made responsible for extending 4 lane paving on Green Valley Road from Safeway to El Dorado Hills Boulevard.

F. Tree Preservation. Precautions shall be made to preserve native oaks to the extent possible with particular concerns for those on the northeast, east and southern edges of the property.

If you have any question about any of the comments and concerns expressed here, please contact APAC President, John Hidahl at: Hidahl@aol.com or (916) 933-2703 or the subcommittee chairman for this project, Ellison Rumsey at: aerumsey@sbcglobal.net or (916) 358-5733.

APAC appreciates having the opportunity to comment on this project.

Sincerely,

John Hidahl
APAC Chairman

cc: Board of Supervisors
George Carpenter, Winn Group
APAC Read File

El Dorado Hills APAC - Non-partisan Volunteers Planning Our Future

El Dorado Hills
Area Planning Advisory Committee
1021 Harvard Way
El Dorado Hills, CA 95762

2011 Board
Chair
John Hidahl
Vice Chair
Jeff Haberman
Secretary/Treasurer
Alice Klinger

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FEB 21 AM 3:42
PLANNING DEPARTMENT

February 20, 2012

El Dorado County Planning Services
Attn: Mel Pabalinas, Project Planner
2850 Fairlane Court
Placerville, CA 95667

Subject: APAC Subcommittee review for Traffic Study for Z 11-0003/ 11 0004 - WINN GP 7 REZONE (Family Real Property LP/Winn Communities/RSC Engineering, Inc.)

Dear Mel,

At the February 8, 2012 APAC meeting, the traffic study (the Study) for the Winn GP 7 Rezone (the Project) was reviewed and continued to the March 14, 2012 APAC meeting. Due to the complexity and the critical need for an accurate traffic study, APAC appointed a subcommittee to review and provide the County with a preliminary assessment of the Study. The following is the Subcommittee's assessment:

First, the APAC Subcommittee concurs with the Study that "*the proposed project (Commercial) is not consistent with the 2004 General Plan land use designation and zoning density for the site (High Density Residential (1-5 DU/acre)). Therefore, a new Cumulative (2025) analysis is required in addition to the analysis already completed for the County's General Plan."*

But more significantly, the Subcommittee believes that the Study <u>fails</u> to accurately assess the major impacts to the Green Valley Corridor traffic-flows that will be caused by the proposed Project. The Study is half right in stating that, "*As defined by the County, the addition of the proposed project to the Existing (2010), Existing plus Approved Projects (2015), and Cumulative (2025) scenarios significantly worsens conditions at multiple study intersections."*

However, the Subcommittee challenges the veracity of the Study's data and its conclusion that the impacts can be successfully mitigated.

The Subcommittee bases its assessment on the following concerns and observations:

1. First, the Subcommittee found arithmetic errors in the Study; specifically Table 1. These inaccuracies put the veracity of the Study itself into question. The appendixes were not included with the study, making it very difficult to determine the accuracy of the study as a whole.

2. In its cumulative projections, the Study failed to include the traffic impacts of the following known development projects in the area: Grande Amis, Alto, La Canada and Dixon Ranch. Jointly and severally, these developments will have major traffic impacts on the GV corridor.

3. The Study's peak hour traffic volumes for the study area were obtained from the county for the years 1998 and 2025[5], and used to calculate five year growth rates. It then used these data for 2015 traffic conditions. *This approach is flawed*: the inherent projected growth rate of 2% completely ignores the housing boom on the western slopes between 2000 and 2005, which was approximately 9 percent.

4. Traffic levels at the major intersections and the projection of traffic patterns from and through the Project are incorrect. A resident's traffic count taken on 1-10-2012 revealed a traffic count significantly more than that reported by the Project. The Subcommittee believes that the already deteriorating traffic level of service (LOS) at the Green Valley Road intersections of Silva Valley, Salmon Falls, and Francisco must not be made worst. Meaningful assessments must be completed, using timely data: An updated traffic study must be completed using 2011 data to properly address the impacts.

5. The Study's recommendation that mitigations for existing LOS F conditions can be achieved by changing green light calibrations at the intersection at Salmon Falls/GV, (Table 9 indicates that the wait time could go from 83 to 49 by simply changing the lighting) is questionable. The Subcommittee believes this in not achievable without major capital improvements to road. *The Subcommittee requests the County immediately test its ability to mitigate traffic impacts by changing the timing of the stop lights at these key intersections before approving any traffic plan for this Project. It should also share results with APAC and the public.*

6. The Subcommittee also believes that "signal cycle length optimization" of this nature may not be a viable solution for improving the LOS at these locations. There was no evidence that the Study considered the impacts of those intersections taking up signal delays at various critical times, to include school hour commute traffic. The general comment of "signal cycle length optimization" does not clearly offer a site specific timing distribution analysis and as such, the general comment that it would mitigate to less than significant cannot be substantiated.

   To be credible, the Study must include an actual signal timing distribution analysis relative to circulations and counts inclusive of school commute traffic for these intersections. Clearly, for example, if the signal cycle time is lengthened for Green Valley Road, then northbound El Dorado Hills Blvd could potentially backup to an unacceptable LOS. This would be true especially in light of the potentially overlapping congestion at El Dorado Hills Blvd at Francisco, which already operates at LOS F at peak hours. This is not a reasonable solution and will not improve the intersection LOS. The County has tried signal cycle length optimizations at other locations unsuccessfully. There are many variables and overlapping considerations to consider when proposing signal cycle length changes.

7. The Subcommittee believes that the proposed lane configuration of southbound Salmon Falls of one left lane, one through lane, and one right turn lane would require extensive intersection modifications.

8. Further, the proposed dedicated right turn lane from southbound Salmon Falls to westbound Green Valley will require signal relocation, significant peak hour cycle length changes affecting other legs (*that were not been analyzed in this analysis*), and the relocation of several utilities at that corner. There may be set back requirements or public utility easements to consider from the adjoining residential properties at the NW corner of intersection. The geometrics to provide a dedicated right turn lane onto westbound Green Valley would require widening; signal relocation and utility relocation were only mentioned qualitatively. This Study must analyze the necessary constructability, right of way, signal timing, lighting and traffic encroachment issues to residential properties bordering this corner before assigning a less than significant impact.

9. Referring to Table TC-2 of the General Plan regarding the volume to capacity ratio of roadway segments (which states: shall not exceed the ratio specified in the GP table), the Subcommittee did find any mention of the capacity ratio in the Study.

10. The Subcommittee would challenge the Study's assumptions about the volume of traffic to be created by the Project. For example, the study projects only one car exiting onto Cambria Way from the Project during peak AM & PM periods. Cambria is the primary neighborhood adjacent to the Project. Clearly, more than one car, AM or PM, will be entering the Project. The committee challenges this number and requests a review by an independent source.

11. Equally as important, the study failed to address the fact that Cambria way is a gated road, adding complexity to the traffic flows and essentially restricting traffic to only one east bound exit point from the Project.

The Subcommittee would also offer the following specific observations and questions:

i. M5 Francisco Drive @ Embarcadero Drive – Is this proposed eastbound right turn flare on eastbound Cambria to southbound Francisco? The Study fails to analyze and mitigate for significant site distance to the south from eastbound Cambria at intersection #5.

ii. Project Access Green Valley location (Figure 2) – Eastbound Green Valley approaching proposed access needs deceleration lane with appropriate taper length per traffic engineer. Will Right of Way need to be acquired for this widening?

iii. M6 El Dorado Hills @ Francisco Drive - This same mitigations given in Mitigation #2 & #3 it fails to address significant constructability, right of way and design issues as they relate to the traffic analysis. This does not address the two lane road capacity on southbound El Dorado Hills Blvd is this assumed to be 4 lanes in this analysis?

iv. Site Distance Evaluation pg. 24 – Was only evaluated at Intersections #7 & #8 proposed site access driveway only! The site distance at intersection #5 is completely deficient. The Study graphically terminates the geometrics immediately south of Cambria. There are significant site distance issues relative to horizontal and vertical curve for traffic making left turn movement out of Cambria onto northbound Francisco nearly impossible. Due to the lane configuration with a grade island between, through, and a right turn from southbound Francisco at Cambria, the distance that cars exiting Cambria would need to cross over to merge into northbound Francisco requires much greater site distance than exists. Site distance to the west is not adequate. There would have to be significant alignment improvements and geometric changes to improve site distance.

v. Plan Access and On Site Circulation pg. 25– the Study should state "Right in and Right out" only and include signing and median delineation called out to affect this. Northbound right turn movements from project access will be blocked by eastbound approaching traffic, it already backs up. Signal timing changes are not thought out adequately in this analysis; there will be significant carry over to other legs of intersections and other intersections that will adversely affect delays and circulation. A simulation need to be done for all improvement and signal timing proposals.

vi. Bicycle and Pedestrian Facilities pg.27 – The Study only specifies on-site requirements. Please provide all pedestrian/ ADA offsite for review. No analysis was provided for offsite in this study. Off-site pedestrian improvements are critical to safety and circulation resulting in the development of this corner.

vii. Onsite traffic circulation (Figure 2) shows 90 degree at access from Green Valley. This design is not to County Design Standards and will not meet fire safe truck turning radius requirements. Please show actual onsite geometrics.

viii. Traffic Plan to Scale- The Subcommittee would like an existing offsite traffic layout and improvements sheet for review. Include full width lanes and intersection inclusive of entire study area for this project and include pedestrian / American Disabilities Act Improvements (ADA) as required.

If you have any question about any of the comments and concerns expressed herein, please contact the Subcommittee chairman, Norm Rowett at: arowett@pacbell.net or (916) 933-2211 or APAC Chairman John Hidahl at: Hidahl@aol.com or 916 933-2703.

The APAC appreciates the opportunity to comment on this Project.

Sincerely,

Norm Rowett,
GVC Subcommittee Chairman, APAC

cc: Planning Commission
George Carpenter, Ellison Rumsey
GVC Subcommittee
APAC Read File

El Dorado Hills
Area Planning Advisory Committee
1021 Harvard Way
El Dorado Hills, CA 95762

2012 Board
Chair
John Hidahl
Vice Chair
Jeff Haberman
Secretary/Treasurer
Alice Klinger

March 14, 2012

El Dorado County Planning Services
Attn: Mel Pabalinas, Project Planner
2850 Fairlane Court
Placerville, CA 95667

Subject: APAC Subcommittee review for Traffic Study for Z 11-0003/ 11 0004 - WINN GP 7 REZONE (Family Real Property LP/Winn Communities/RSC Engineering, Inc.)

Dear Mel,

The traffic study (the Study) for the *Winn GP 7 Rezone* (the Project) was reviewed at the APAC's March 14, 2012 monthly meeting. Also, the APAC's Green Valley Corridor Subcommittee met on February 27, with the Winn's agent, the Kimely-Horn traffic engineer, and Winn's consulting engineer.

The APAC committee after reviewing the Study and meeting with the Winn representatives, voting X to Y, found that the Study failed to fairly or accurately profile current traffic conditions in the impacted area, and failed to offer credible solutions for mitigating the traffic impacts of the commercial center if it were approved.

The APAC committee did concur with the Study that, quote, "...*the proposed project (Commercial) is not consistent with the 2004 General Plan land use designation and zoning density for the site (High Density Residential (1-5 DU/acre)). Therefore, a new Cumulative (2025) analysis is required in addition to the analysis already completed for the County's General Plan.*"

The APAC also concurs with the Study that, quote, "*As defined by the County, the addition of the proposed project to the Existing (2010), Existing plus Approved Projects (2015), and Cumulative (2025) scenarios significantly worsens conditions at multiple study intersections.*"

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APAC bases its assessment on the following concerns and observations:

1. First, the committee found arithmetic errors in the Study; specifically Table 1. These inaccuracies call into question the veracity of the Study's presentation of data. The appendices were not included with the study, making it very difficult to determine the accuracy of the study as a whole.

2. In its cumulative projections, the Study failed to include the serious traffic impacts of the following known development projects in the area: Grande Amis, Alto, La Canada and Dixon Ranch. Jointly and severally, these developments will have major traffic impacts on the Green Valley Road (GVR) corridor.

3. The Study omitted glaring sight-distance safety liabilities to an already substandard intersection, Cambria Way and Francisco. Cambria intersects with one of the two driveways in/out of proposed commercial center. With non-standard geometrics and limited sight-distance to the south, this intersection fails to meet county standards in its *present* configuration.

   According to the "Transportation and Circulation Element" document, Table 9-6, Design Intersection Sight Distance, AASHTO requires 500' of sight distance in this condition; specifically where a vehicle is turning left from a stop sign (Cambria) onto a major road (Francisco). The report skirts the issue by declaring that "According to AASHTO, an assumed 45 mph design speed (40 mph posted speed limit) requires a minimum of 360-feet of Stopping Sight Distance (SSD). The KHA traffic engineer acknowledged at the Subcommittee meeting that the correct metric to apply is a sight-distance of 500', but stated that the DOT, as an expert, is aware of the correct measure to use.

   Bottom-line, notwithstanding the Study's failed assumptions, the corner of Francisco/Cambria, as it currently operates, is hazardous and <u>does not meet AASHTO standards.</u>

   It's significant to note: The proposed commercial development is estimated to add 3,388 daily trips to the impacted intersections. Assuming, conservatively, that 35% of those trips use the Cambria entry, this project will be channeling an additional 1,186 trips through an intersection that <u>already</u> fails to meet standards.

   Anecdotally, residents expressed grave concerns about existing driving conditions: "...nearly impossible to cross Francisco from Cambria at peak hours today...any additional traffic or widening of Francisco to accommodate greater traffic volumes will make crossing Francisco more difficult and dangerous".

4. The Study fails to adequately address the serious traffic problem where the commercial center proposes an in/out driveway onto Green Valley Road. The Project's second driveway poses challenging traffic circulation with a right-in/right-out only from Green Valley Road. This intersection will result in a non standard intersection. The concern is that customer traffic will attempt to exit left out onto Green Valley, requiring a left-cross-traffic turn at the right-in/right-out driveway. This is a serious issue not addressed by the Study.

5. The Study fails to realistically address impacts to pedestrians at Cambria and Francisco. There is a pedestrian asphalt walkway which currently is about 8ft behind the stop sign. The January 6, 2012 memo from Matt Weir, the Kimely-Horn traffic engineer, states (on page 5), "all Cambria

Way pedestrian traffic should be confined to the north side of the road, eliminating the need to cross Cambria Way at the Francisco Drive intersection..."

This is not realistic, nor prudent. No offsite pedestrian plan or improvements are proposed (or funded) to remove existing pedestrian walk ways. The proposed commercial development is estimated to add 3,388 daily trips to the complex. Assuming that a conservative 35% of those trips use the Cambria entry that means that this project will be funneling an additional 1,186 trips through an intersection that already doesn't meet standards.

6. The Study's recommendation that mitigations for existing LOS F conditions can be achieved by changing green light calibrations at the intersection at Salmon Falls/GVR, (Table 9 indicates that the wait time could go from 83 to 49 by simply changing the lighting) *is questionable*. The Study states that the signal timing could mitigate traffic impacts, yet g\offers no concrete analysis, e.g., the industry standard simulation. *The APAC requests a signal simulation to prove timing could mitigate.*

   The Study's comment of signal cycle length optimization does not offer a site specific timing distribution analysis; it cannot be substantiated without actual signal timing distribution analysis relative to circulations and counts, inclusive of school commute traffic, for this intersection. If signal cycle time is lengthened for Green Valley Road then northbound El Dorado Hills Blvd could potentially backup to an unacceptable LOS, especially in light of the potentially overlapping congestion at El Dorado Hills Blvd at Francisco, which already operates at LOS F at peak hours.

   This is not a reasonable solution and will not improve the intersection LOS. The County has tried signal cycle length optimizations at other locations unsuccessfully. There are many variables and overlapping considerations to consider when proposing signal cycle length changes.

7. The Study's peak hour traffic data were not reflective of reality. Data volumes for the study area were obtained from the county for the years 1998 and 2025[5], and used to calculate five year growth rates. The Study then used these data for 2015 traffic conditions. Per KHA, existing (2010) Conditions' actual counts for 2005 – 2007 were increased using a 2% annual growth rate. This approach seemed flawed: the inherent projected growth rate of 2% completely ignores the housing boom on the western slopes between 2000 and 2005, which was <u>approximately 9 percent.</u>

   Example: The traffic counts used for the corner of Green Valley/Francisco do not seem to be representative of the actual. Dowling Associates outlined the Scope of Work for the Study and in a memo dated November 22, 2010 stated that "where counts are older than 2007, new counts should be conducted. For counts that were conducted in 2007 or later should be adjusted [sic] to reflect existing conditions by using an average annual growth rate of 2%..."

   Kimley-Horn re-counted traffic on two intersections. The growth rate of 2% was applied to all others, including the corner of Francisco and Green Valley, where the baseline was completed in 2005. Anecdotally, an informal survey of residents of the Promontory evinced that most residents moved into that area in 2006. The Assistant Manager of Safeway at Francisco verified that the store opened in December of 2006. It does not make sense to apply a low growth rate to a traffic count that predates the 2007 guideline by two years; particularly when this intersection absorbed the combination of a new shopping center and two large new residential developments completed after that time!

8. Traffic levels at the major intersections and the projection of traffic patterns from and through the Project are incorrect. A resident's traffic count taken on 1-10-2012 revealed a traffic count significantly more than that reported by the Project. APAC believes that the already deteriorating traffic level of service (LOS) at the Green Valley Road intersections of Silva Valley, Salmon Falls, and Francisco must not be made worst. Meaningful assessments must be completed, using timely data: An updated traffic study must be completed using 2011 data to properly address the impacts.

9. The committee believes that the proposed lane configuration of southbound Salmon Falls of one left lane, one through lane, and one right turn lane would require extensive intersection modifications.

10. Further, the proposed dedicated right turn lane from southbound Salmon Falls to westbound Green Valley will require signal relocation, significant peak hour cycle length changes affecting other legs (*that were not been analyzed in this analysis*), and the relocation of several utilities at that corner. There may be set back requirements or public utility easements to consider from the adjoining residential properties at the NW corner of intersection. The geometrics to provide a dedicated right turn lane onto westbound Green Valley would require widening; signal relocation and utility relocation were only mentioned qualitatively. This Study must analyze the necessary constructability, right of way, signal timing, lighting and traffic encroachment issues to residential properties bordering this corner **before** assigning a less than significant impact.

11. Referring to Table TC-2 of the General Plan regarding the volume to capacity ratio of roadway segments (which states: shall not exceed the ratio specified in the GP table), the Subcommittee did not find any mention of the capacity ratio in the Study.

12. The committee would challenge the Study's assumptions about the volume of traffic to be created by the Project. For example, the study projects only one car exiting onto Cambria Way from the Project during peak AM & PM periods. Cambria is the primary neighborhood adjacent to the Project. Clearly, more than one car AM or PM will be entering the Project. The committee challenges this number and requests a review by an independent source.

13. Equally as important, the study failed to address the fact that Cambria way is a gated road, adding complexity to the traffic flows and essentially restricting traffic to only one east bound exit point from the Project.

The committee would also offer the following specific observations and questions:

i. M5 Francisco Drive @ Embarcadero Drive – Is this proposed eastbound right turn flare on eastbound Cambria to southbound Francisco? The Study fails to analyze and mitigate for significant site distance to the south from eastbound Cambria at intersection #5.

ii. Project Access Green Valley location (Figure 2) – Eastbound Green Valley approaching proposed access needs deceleration lane with appropriate taper length per traffic engineer. Will Right of Way need to be acquired for this widening?

iii. M6 El Dorado Hills @ Francisco Drive - This same mitigations given in Mitigation #2 & #3 it fails to address significant constructability, right of way and design issues as they relate

to the traffic analysis. This does not address the two lane road capacity on southbound El Dorado Hills Blvd is this assumed to be 4 lanes in this analysis?

iv. Site Distance Evaluation pg. 24 – Was only evaluated at Intersections #7 & #8 proposed site access driveway only! The site distance at intersection #5 is completely deficient. The Study graphically terminates the geometrics immediately south of Cambria. There are significant site distance issues relative to horizontal and vertical curve for traffic making left turn movement out of Cambria onto northbound Francisco nearly impossible. Due to the lane configuration with a grade island between, through, and a right turn from southbound Francisco at Cambria, the distance that cars exiting Cambria would need to cross over to merge into northbound Francisco requires much greater site distance than exists. Site distance to the west is not adequate. There would have to be significant alignment improvements and geometric changes to improve site distance.

v. Plan Access and On Site Circulation pg. 25– the Study should state "Right in and Right out" only and include signing and median delineation called out to affect this. Northbound right turn movements from project access will be blocked by eastbound approaching traffic, it already backs up. Signal timing changes are not thought out adequately in this analysis; there will be significant carry over to other legs of intersections and other intersections that will adversely affect delays and circulation. A simulation need to be done for all improvement and signal timing proposals.

vi. Bicycle and Pedestrian Facilities pg.27 – The Study only specifies on-site requirements. Please provide all pedestrian/ ADA offsite for review. No analysis was provided for offsite in this study. Off-site pedestrian improvements are critical to safety and circulation resulting in the development of this corner.

vii. Onsite traffic circulation (Figure 2) shows 90 degree at access from Green Valley. This design is not to County Design Standards and will not meet fire safe truck turning radius requirements. Please show actual onsite geometrics.

viii. Traffic Plan to Scale-The Subcommittee would like an existing offsite traffic layout and improvements sheet for review. Include full width lanes and intersection inclusive of entire study area for this project and include pedestrian / American Disabilities Act Improvements (ADA) as required.

If you have any question about any of the comments and concerns expressed herein, please contact the Subcommittee chairman, Norm Rowett at: arowett@pacbell.net or (916) 933-2211 or APAC Chairman John Hidahl at: Hidahl@aol.com or 916 933-2703.

The APAC appreciates the opportunity to comment on this Project.

Sincerely,

John Hidahl

John Hidahl,
Chairman, APAC

cc: Planning Commission
George Carpenter, Ellison Rumsey
GVC Subcommittee
APAC Read File

El Dorado Hills APAC - Non-partisan Volunteers Planning Our Future